

# AGENDA

## Local Traffic Committee



*We're with you*

Thursday 16 May 2024

Council Chambers | Wingecarribee Shire Council Civic Centre  
68 Elizabeth Street, Moss Vale at 10:00 am

**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**

---

<b>1 OPENING OF THE MEETING .....</b>	<b>4</b>
<b>2 ACKNOWLEDGEMENT OF COUNTRY .....</b>	<b>4</b>
<b>3 APOLOGIES.....</b>	<b>4</b>
<b>4 ADOPTION OF MINUTES OF PREVIOUS MEETING.....</b>	<b>4</b>
<b>5 DECLARATIONS OF INTEREST.....</b>	<b>4</b>
<b>6 AGENDA REPORTS .....</b>	<b>6</b>
6.1 Police Report On Recent Road Crashes In The Shire .....	6
6.2 Proposed Changes To Parking Arrangements On Argyle Street, Berrima .....	7
6.3 Intersection Of Ellsmore Road And Blue Gum Road Bundanoon - Give Way Restriction. ....	12
6.4 Range Road Glenquarry - Road Reconstruction Including Signs And Line Marking.....	15
6.5 Market Place, Berrima - Line Marking For 45° Angle Parking Spaces.....	22
6.6 2024 Bowral Classic.....	27
6.7 Proposed Pedestrian Crossing - Lackey Road, Moss Vale.....	29
6.8 Railway Avenue, Wingello - Removal Of Pedestrian Refuge.....	35
6.9 Access Road Arrangements For 6 Lot Subdivision At 267 Bundanoon Road Exeter .....	40
6.10 Traffic Arrangements For Proposed Development On Sally Corner Road, Sutton Forest.....	65
<b>7 DATE OF NEXT MEETING .....</b>	<b>86</b>
<b>8 MEETING CLOSURE .....</b>	<b>86</b>

**Our Mission, Our Vision, Our Values**

**OUR MISSION**

To create and nurture a vibrant and diverse community growing and working in harmony with our urban, agricultural and natural environments

**OUR VISION**

**Leadership:** *'An innovative and effective organisation with strong leadership'*

**People:** *'A vibrant and diverse community living harmoniously, supported by innovative services and effective communication with Council'*

**Places:** *'Places that are safe, maintained, accessible, sympathetic to the built and natural environment, that supports the needs of the community'*

**Environment:** *'A community that values and protects the natural environment enhancing its health and diversity'*

**Economy:** *'A strong local economy that encourages and provides employment, business opportunities and tourism'*

**OUR VALUES**

Integrity, trust and respect

Responsibility and accountability

Communication and teamwork

Service quality

## **1 OPENING OF THE MEETING**

The Senior Traffic Engineer will open the meeting.

## **2 ACKNOWLEDGEMENT OF COUNTRY**

“Wingecarribee Shire Council acknowledges the Gundungurra and Tharawal people as the traditional custodians of this land we now call the Wingecarribee Shire. I pay my respect to Elders both past, present and emerging. I would also like to extend that respect to all Aboriginal and Torres Strait Islanders present here today.”

## **3 APOLOGIES**

Nil at time of print.

## **4 ADOPTION OF MINUTES OF PREVIOUS MEETING**

That the minutes of the Traffic Committee meeting held on 21 March 2024, recommendation numbers TC 2024/11 to TC 2024/21 inclusive, copies of which were forwarded to Committee members, have been received and noted.

## **5 DECLARATIONS OF INTEREST**

The provisions of Chapter 14 of the Local Government Act 1993 regulate the way in which nominated staff of Council conduct themselves to ensure that there is no conflict between their private interests and their public trust.

The Act prescribes that where a member of Council (or a Committee of Council) has a direct or indirect financial (pecuniary) interest in a matter to be considered at a meeting of the Council (or Committee), that interest and the reasons for declaring such interest must be disclosed as soon as practicable after the start of the meeting.

As members are aware, the provisions of the Local Government Act restrict any member who has declared a pecuniary interest in any matter from participating in the discussions or voting on that matter and further require that the member vacate the Chamber.

Council’s Code of Conduct provides that if members have a non-pecuniary conflict of interest, the nature of the conflict must be disclosed. The Code also provides for a number of ways in which a member may manage non pecuniary conflicts of interest.

**Where necessary any Councillor, Committee Member and nominated staff of Council should disclose any interest and the reason for declaring such interest in the matters under consideration at this meeting.**



**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**

---

Councillors and Committee Members are requested to complete the appropriate form to be handed up at the Meeting.

## **6 AGENDA REPORTS**

---

### **6.1 Police Report on recent road crashes in the Shire**

---

**Report Author: Traffic Engineer**

**Authoriser: Karin Targa**

#### **PURPOSE**

---

Reporting on recent road crashes in the Shire recorded by Police.

#### **RECOMMENDATION**

---

***THAT the information be received and noted.***

#### **REPORT**

---

##### **BACKGROUND**

At its meeting on 28 April 2010, Council adopted the recommendation of the Traffic Committee as follows:

THAT the Police report to each Traffic Committee meeting any fatal and serious injury crashes and locations where a pattern of crashes is developing AND THAT these crashes also be reported to Council staff to permit a timely investigation of the road environment.

#### **CONCLUSION**

---

That the information be received and noted.

#### **ATTACHMENTS**

---

Nil

---

## **6.2 Proposed Changes to Parking Arrangements on Argyle Street, Berrima**

---

**Report Author: Traffic Engineer**

**Authoriser: Karin Targa**

### **PURPOSE**

---

To amend the parking restrictions on Argyle Street, Berrima, adjacent to the Berrima Gaol.

### **RECOMMENDATION**

---

***THAT the 70m No Parking zone on the west side of Argyle Street, Berrima, adjacent to the Berrima Gaol be amended to include:***

- **Removal of the existing Bus Zone at the southern end of Argyle Street**
- **The installation of a 32m Bus Zone south of the Berrima Gaol driveway**
- **The creation of 58m of unrestricted parking south of the proposed Bus Zone**
- **Removal of the existing 22m Loading Zone located opposite the driveway to Berrima Gaol**

### **REPORT**

---

There is currently a 70m No Parking zone on Argyle Street, Berrima, between the Bus Zone to the south and 4m north of the Berrima Gaol access driveway.

Berrima Gaol is no longer in operation with the gaol being permanently closed in 2020.

This report proposes to remove the Bus Zone at the southern end of Argyle Street and create a 32m Bus Zone south of the driveway to Berrima Gaol. The terrain at the existing Bus Zone is uneven and unsafe for people to step on when entering and exiting the bus.

The 70m of No Parking will be removed. Removal of the No Parking Zone will allow for 58m of unrestricted parking.

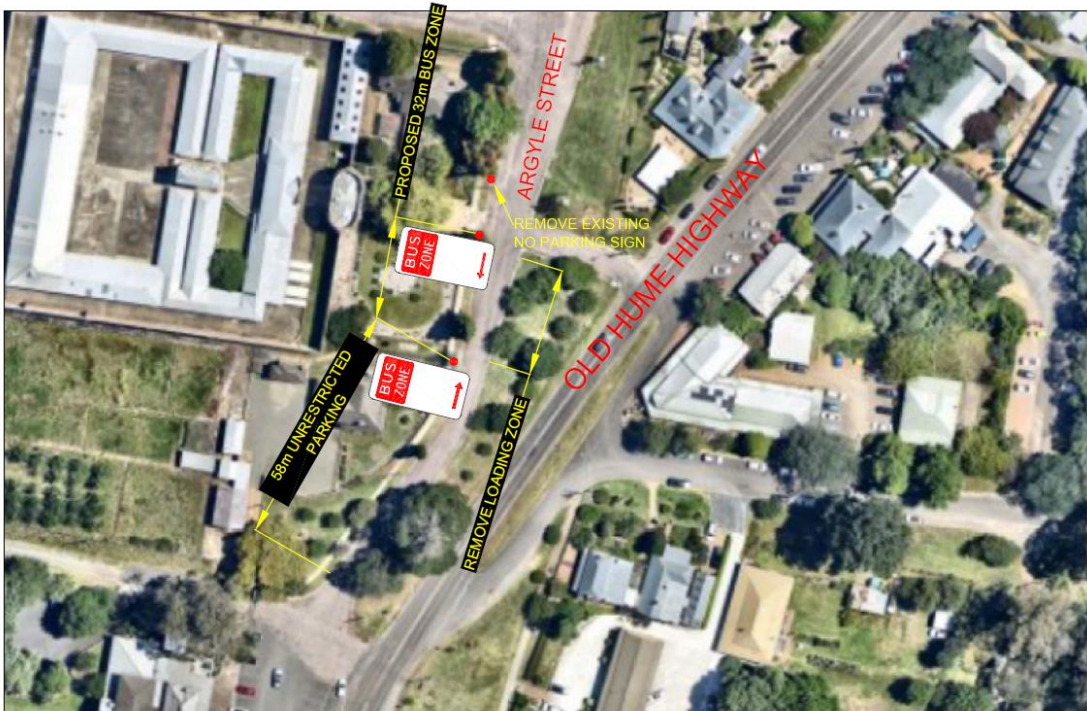
The 22m Loading Zone on the east side of Argyle Street would be removed. The Loading Zone was created in 2017 to allow for deliveries when the gaol was in operation. The width of Argyle Street through a portion of the loading zone is narrow. Vehicles parked on both sides of Argyle Street would not allow enough width for a vehicle to pass.



**EXISTING PARKING LAYOUT  
ARGYLE STREET, BERRIMA**

**Image 1 – Existing parking layout on Argyle Street**





**PROPOSED PARKING LAYOUT  
ARGYLE STREET, BERRIMA**

**Image 2 – Proposed parking layout on Argyle Street**





**Image 3 – Existing Loading Zone**



**Image 4 – Existing Bus Zone**



**Image 5 – Existing No Parking Zone**

**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**

---

**CONCLUSION**

The proposed parking changes to Argyle Street Berrima will allow for up to 58m of extra space for vehicles to park. This can provide approximately 10 - 11 new parking spaces.

The proposed Bus Zone will provide a safer terrain for people to use when entering and exiting the bus.

The Loading Zone is not required due to Berrima Gaol no longer being in operation.

The proposed parking arrangements are recommended for approval.

**ATTACHMENTS**

---

Nil

---

### **6.3 Intersection of Ellsmore Road and Blue Gum Road Bundanoon - Give way restriction.**

---

**Report Author: Traffic Engineer**

**Authoriser: Karin Targa**

#### **PURPOSE**

---

To advise and have the Committee's approval for recently installed Give Way sign and line marking at Blue Gum Road with the intersection of Ellsmore Road, Bundanoon.

#### **RECOMMENDATION**

---

***THAT the Council approves:***

- 1. The installation of a Give way sign, 9m TB Line and TB1 Line,***
- 2. Existing unbroken dividing single central barrier line (9m) in Blue Gum Road be replaced with 10m unbroken double dividing barrier (BB) lines in Blue Gum Road.***
- 3. A 30m TB1 line be installed to delineate the priority vehicles movement on Ellsmore Road***
- 4. 15m Double Barrier lines at either end of the TB1 Curve delineation line on Ellsmore Road***

#### **REPORT**

---

##### **BACKGROUND**

A recent search confirmed that there is not enough information of the installation of existing signs and lines in the intersection of Blue Gum Road and Ellsmore Road in Council's records. The existing Give Way sign and line marking installation was completed in early of March 2023, in the subject intersection. It was not included in Traffic Committee agenda items previously.

##### **REPORT**

The intersection of Ellsmore Road and Blue Gum Road has been inspected. In this location there is a right-angled bend at Ellsmore Road, and Blue Gum Road forming a three-legged T-type intersection. Blue Gum Road is a No Through local road with a very low volume of traffic. The major traffic flow on Ellsmore Road has the priority in this intersection. The existing Give Way sign and line marking in Blue Gum Road are justified in present traffic condition.

Currently a TB line (9m), and an unbroken central barrier line (9m) are in place. The unbroken dividing single central barrier line (9m) in Blue Gum Road will be replaced with a 10m unbroken double dividing barrier (BB) line in Blue Gum Road in combination with Give Way sign.

The double barrier lines are to be place at the centre of the road. The road width for three legs of the intersection is 6m.



**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**

---

A dashed line to delineate the priority turn is proposed to be installed on Ellsmore Road with double barrier lines at either end of the turn line.

**CONCLUSION**

---

The traffic flow of Blue Gum Road needs to be controlled by installing a Give Way restriction in it. The location of new Give Way sign will have minimum impact on adjoining residents.

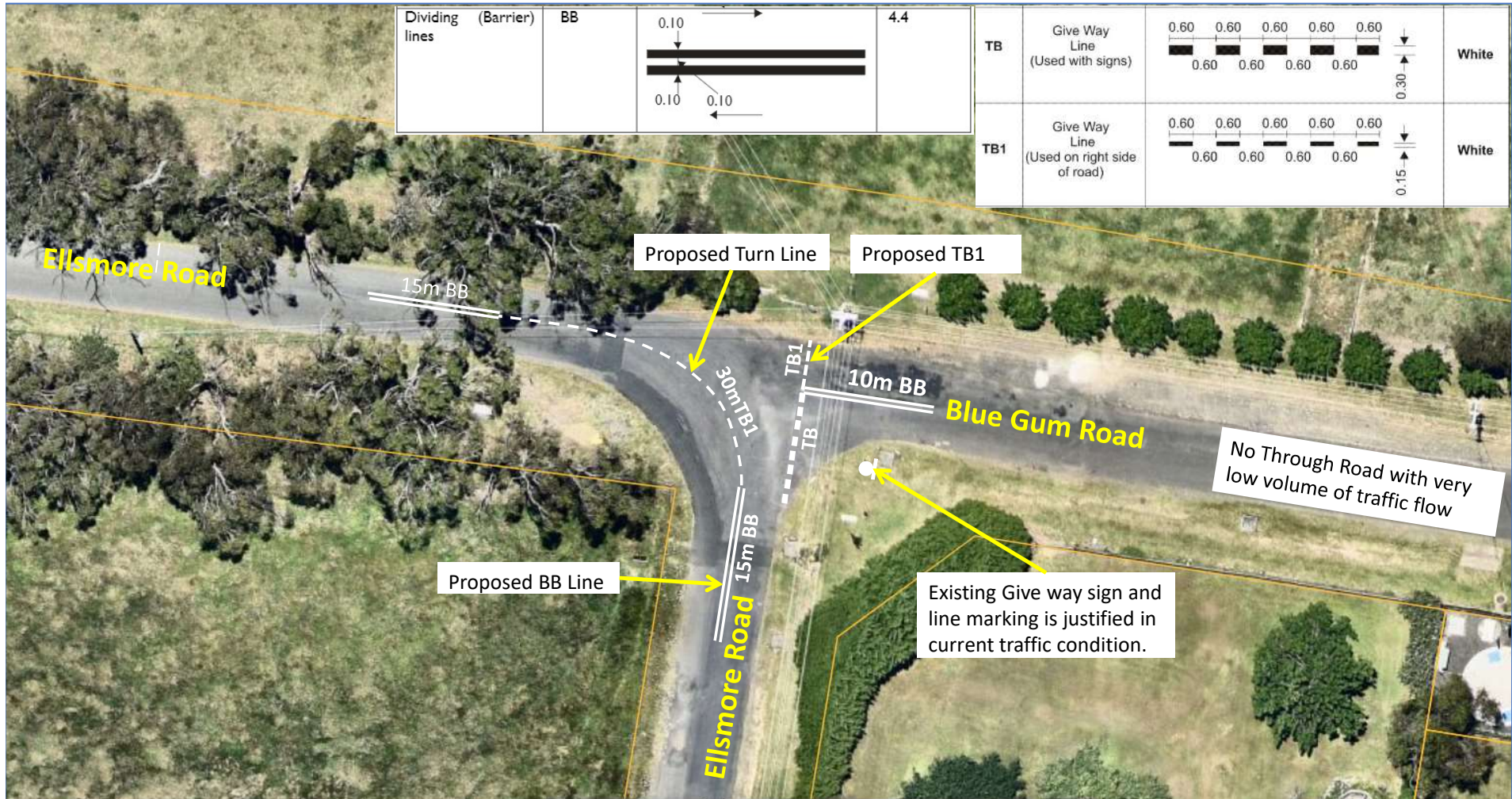
It is recommended that Council approves the recent installation of a Give Way sign and associated line marking, in Blue Gum Road at the intersection of Ellsmore Road Bundanoon.

**ATTACHMENTS**

---

1. Blue Gum Ellsmore Road Give Way Sign and Line Marking [6.3.1 - 1 page]

**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**



---

## **6.4 Range Road Glenquarry - Road reconstruction including signs and line marking.**

---

**Report Author: Traffic Engineer**

**Authoriser: Karin Targa**

### **PURPOSE**

---

To have the Committee's approval for the proposed road reconstruction with Signage and Line marking in the section of Range Road from George Emery Lane to Tourist Road, Glenquarry.

### **RECOMMENDATION**

---

***THAT Council approves proposed review and upgrading:***

- ***Installation of new signs R4 – 1(B) on both direction at CH 1920 in Range Road, Glenquarry (Ref. Sheet 74 of 74).***
- ***Installation of new BB line marking with RRPMs from:***
  - a) ***CH465 – CH550 at 12m centres (Ref. Sheet 72 of 74)***
  - b) ***CH850 – CH925 at 12m centres (Ref. Sheet 72 of 74)***
  - c) ***CH1065 – CH1130 at 12m centres (Ref. Sheet 73 of 74)***
  - d) ***CH1235 – CH1290 at 12m centres (Ref. Sheet 73 of 74)***
  - e) ***CH1540 – CH1605 at 12m centres (Ref. Sheet 74 of 74)***
  - f) ***CH1740 – CH1890 at 12m centres (Ref. Sheet 74 of 74)***

### **REPORT**

---

#### **BACKGROUND**

Range Road is a two-lane sealed road with a speed limit of 60 km/hr. It has a traffic count of 607 average annual daily traffic and a heavy vehicle percentage of 10.28%.

Sections of the road pavement have been found to be in very poor condition including poor drainage, failed asphalt surface and narrow road width.

This road section requires new pavement for approximately 2 km between George Emery Lane to Tourist Road. The works include the installation of new stormwater drainage and subsoil drainage, fence relocation, service relocations, dish gutter construction, vegetation removal, shoulder widening, signs, line marking and associated works.

#### **REPORT**

Range Road is a local road located 5 km southeast of Bowral and Glenquarry. Range Road has recently had improvement works undertaken.

## **AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING THURSDAY 16 MAY 2024**

---

The works will include:

- Subsoil drainage installation.
- Stormwater pit and pipe installations
- Concrete dish gutter construction
- Fence and service relocations.
- Road pavement reconstruction including signs and line marking.
- Vegetation removal

A set of plans for the road reconstruction has been prepared by Council's Project Delivery team. The Project Delivery team have requested Traffic Committee to endorse proposed signage & line marking as part of the road improvements.

The proposed road works will have a post construction pavement width of 7m. The line marking is proposed for the bends to improve road safety by helping prevent vehicles cutting the corner, and to better delineate the bends.

Range Road currently does not have any line marking. The centreline marking proposed in this report will be new.

Design Plans showing the road improvements, including signage and line marking are attached.

### **CONCLUSION**

---

It is recommended that:

- Installation of new signs R4 – 1(B) on both direction at CH 1920 in Range Road, Glenquarry (Ref. Sheet 74 of 74).
- Installation of BB line with RRPMS from:
  - a) CH465 – CH550 at 12m centres (Ref. Sheet 72 of 74)
  - b) CH850 – CH925 at 12m centres (Ref. Sheet 72 of 74)
  - c) CH1065 – CH1130 at 12m centres (Ref. Sheet 73 of 74)
  - d) CH1235 – CH1290 at 12m centres (Ref. Sheet 73 of 74)
  - e) CH1540 – CH1605 at 12m centres (Ref. Sheet 74 of 74)
  - f) CH1740 – CH1890 at 12m centres (Ref. Sheet 74 of 74)

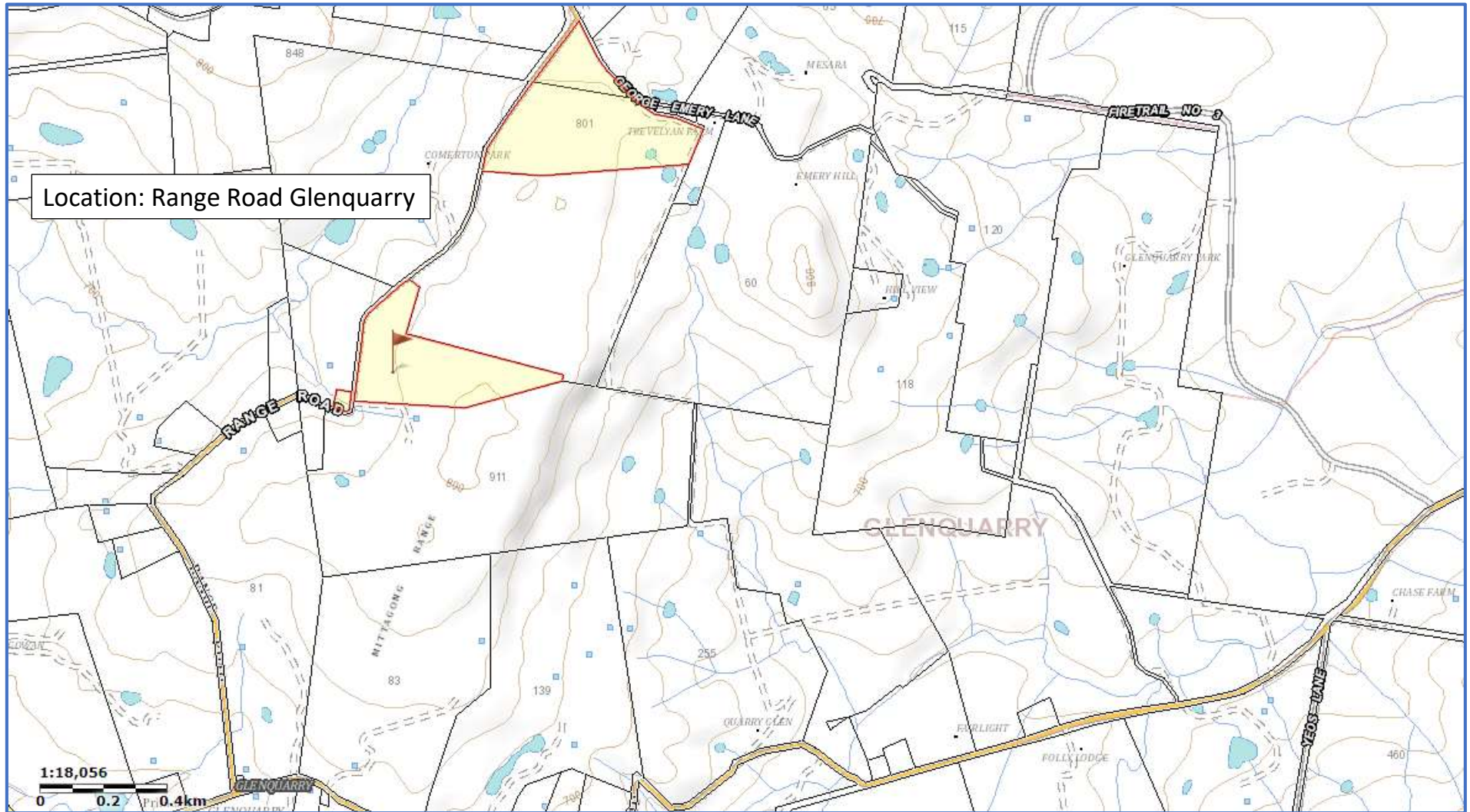
### **ATTACHMENTS**

---

1. Range Road Glenquarry - Road reconstruction including signs and line marking. [6.4.1 - 5 pages]



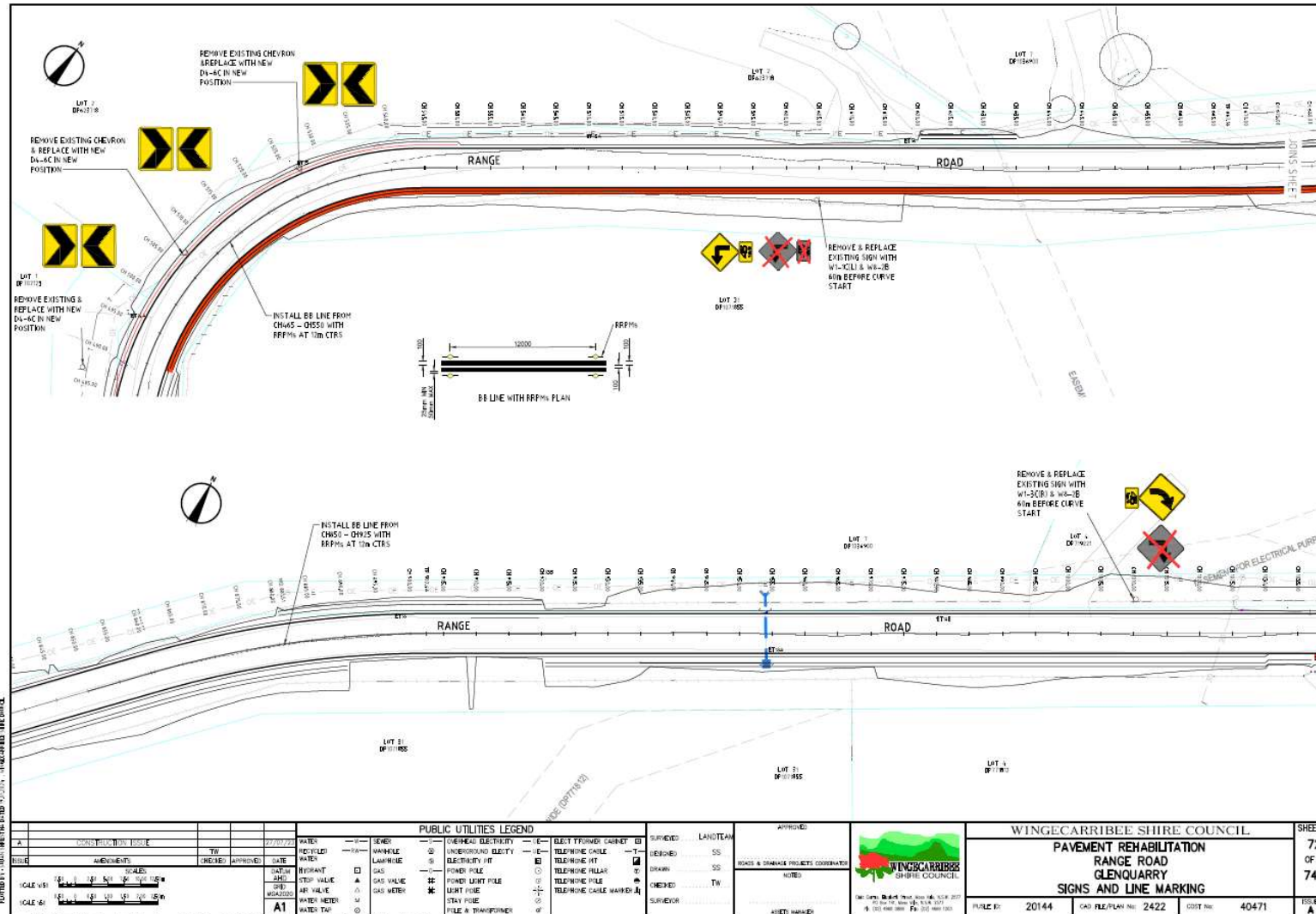
**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**



**6.4.1 Range Road Glenquarry - Road reconstruction including signs and line marking.**



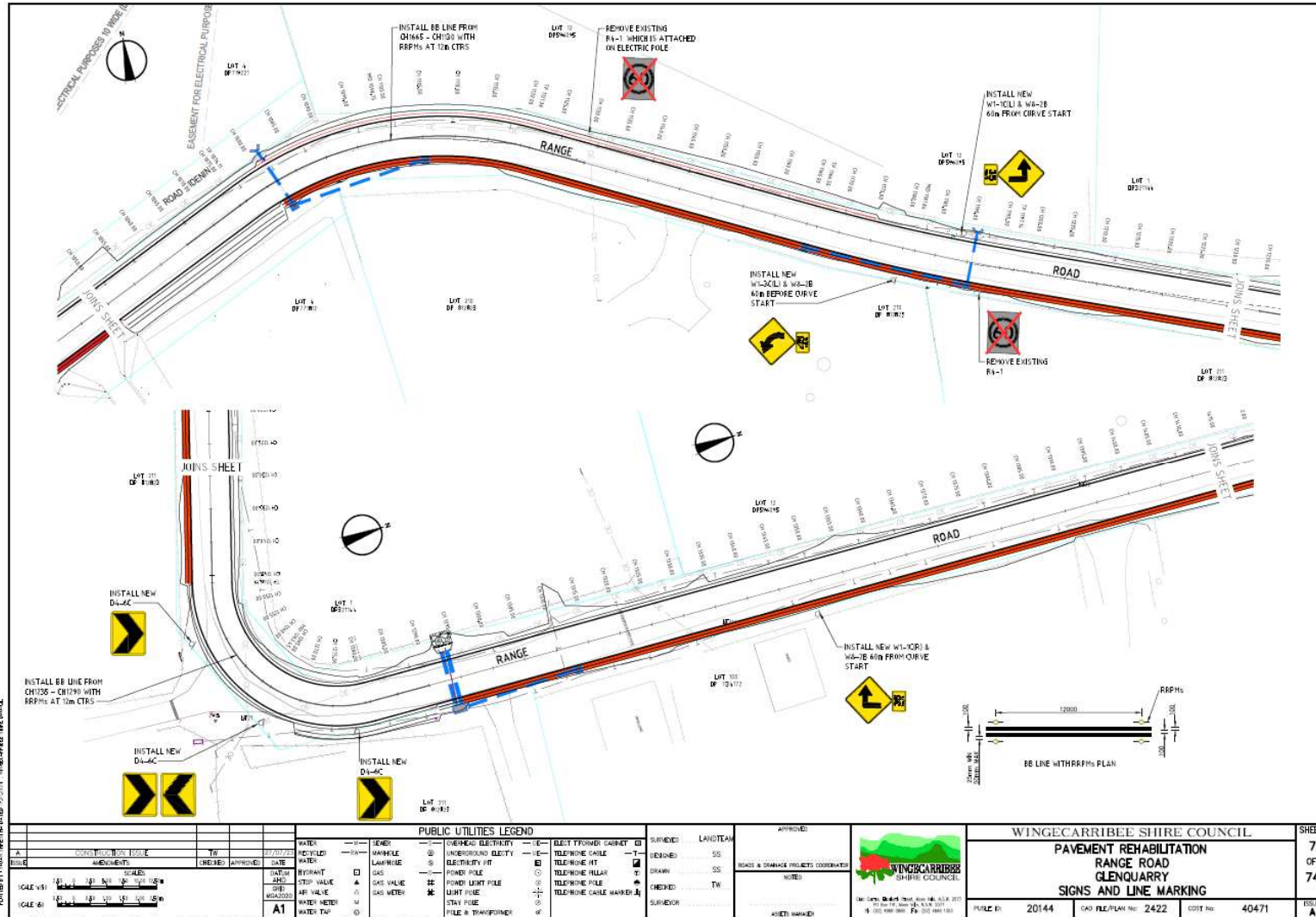
**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**



**6.4.1 Range Road Glenquarry - Road reconstruction including signs and line marking.**



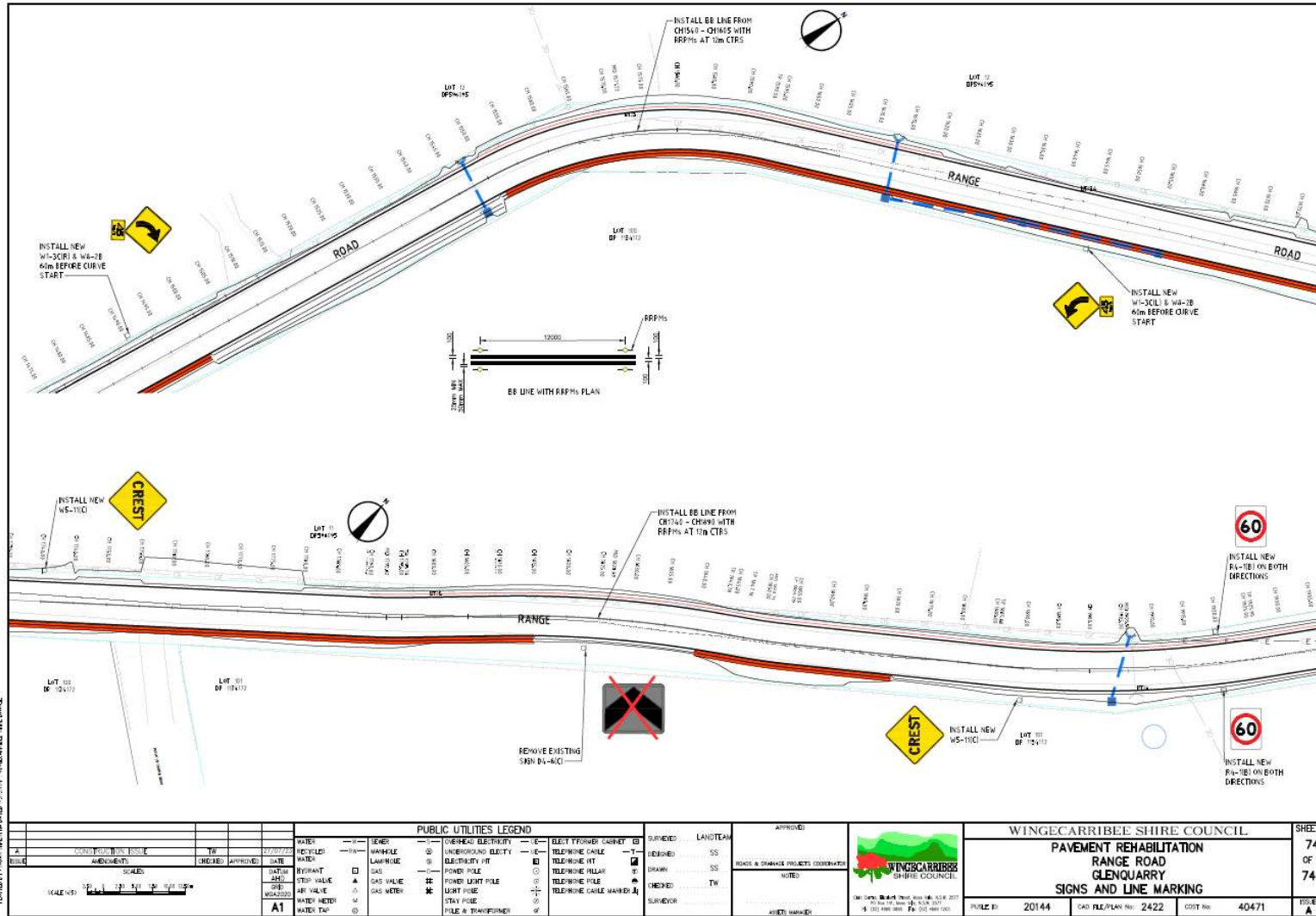
**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**



**6.4.1 Range Road Glenquarry - Road reconstruction including signs and line marking.**



**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**



**6.4.1 Range Road Glenquarry - Road reconstruction including signs and line marking.**

---

## **6.5 Market Place, Berrima - Line marking for 45° angle parking spaces.**

---

**Report Author:** Traffic Engineer

**Authoriser:** Karin Targa

### **PURPOSE**

---

To advise and have Traffic Committee's approval for the recent painting for new line marking of existing unmarked 45° angle rear to the kerb parking spaces as FIGURE 2.2 LAYOUTS FOR ANGLE PARKING SPACES defined in AS/NZS 2890.1 :2004, on the northern kerb side of Market Place, Berrima.

### **RECOMMENDATION**

---

***THAT the line marking for existing unmarked 45° angle rear to the kerb parking spaces as FIGURE 2.2 LAYOUTS FOR ANGLE PARKING SPACES defined in AS/NZS 2890.1 :2004, on the northern kerb side of Market Place, Berrima is recommended for approval.***

### **REPORT**

---

#### **BACKGROUND**

Council received a request from the community to line mark the existing unmarked 45° angle rear to the kerb parking spaces on the northern kerb side of the Market Place, Berrima.

The existing 45° parking area has signage to indicate the parking restriction but no line marking is present.

#### **REPORT**

The parking area on Market Place, Berrima, is sign posted to specify 45° Angle Parking, Rear to Kerb, Vehicles Under 6m Only. The pavement width of this section of Market Place is 13m which includes the angle parking area.

A layout plan for the 45° angle line marking is attached.

The purpose of the line marking is to provide a better delineated 45° parking zone to create more efficient and orderly parking, and better compliance with the designated angle parking.

The line marking will provide positive benefits in that it will increase parking capacity with more efficient parking as vehicles are guided to park in manner that reduces the spacing between vehicles.

Vehicles often do not adhere to the sign posted 45° angle parking. Vehicles tend to park closer 90° or angle park in the wrong direction. This haphazard parking can be a safety issue as vehicles back into spaces with no guidance of the correct parking arrangement.

The proposed angle parking line marking complies with Australian Standards



**Image 1 – Existing 45° angle parking zone with no line marking**



**Image 2 – This image captures vehicles not adhering to existing non line marked 45° parking area on Market Place**

Angle parking line marking already exists on the Old Hume Highway in Berrima. The line marking outlined in this report is like the existing line marking on Old Hume Highway through the CBD of Berrima.





**Image 2 – Existing 45° angle line marking on Old Hume Highway Berrima**

## **CONCLUSION**

---

The line marking will better define the intended angle parking arrangement and lead to more efficient and orderly parking on Market Place.

The line marking will support the existing 45° angle rear to the kerb parking spaces on the northern kerb side of Market Place, creating more efficient parking.

The 45° angle parking line marking on Market Place, Berrima is recommended for approval.

## **ATTACHMENTS**

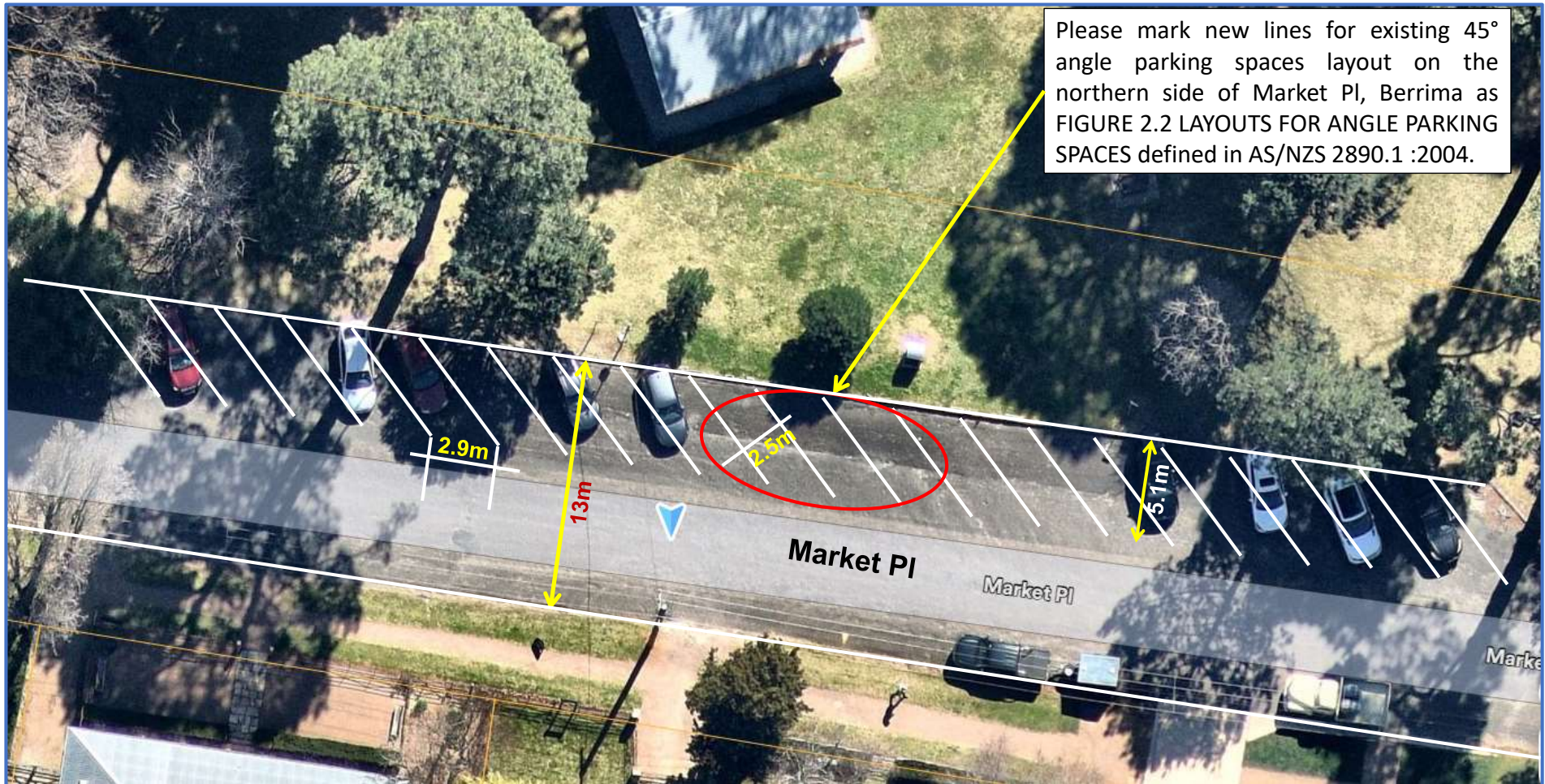
---

1. Market Place, Berrima - Line marking for 45° angle parking spaces [6.5.1 - 2 pages]

**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**









---

## **6.6 2024 Bowral Classic**

---

**Report Author: Traffic Engineer**

**Authoriser: Karin Targa**

### **PURPOSE**

---

Reporting on the traffic management arrangements for the 2024 Bowral Classic event.

### **RECOMMENDATION**

---

**THAT there is no objection to the traffic arrangements proposed for the 2024 Bowral Classic events on Saturday 19 October and Sunday 20 October subject to approval by the NSW Police and completion of the approval process for Class 2 Race Events as per the NSW Guidelines for Bicycle Road Races and the Guide to Traffic and Transport Management for Special Events including the approval of specified speed reductions and granting of Road Occupancy Licenses (ROL) by TfNSW.**

### **REPORT**

---

The seventh instalment of the Bowral Classic will take place on Saturday 19 October and Sunday 20 October 2024.

The Bowral Classic is a road cycling event that takes place on roads throughout the Wingecarribee Shire. The event includes three courses, the 150km Maxi Classic, the 120km Challenge Classic, and the 85km Rouleurs Classic. The ride starts at Bong Bong St, Bowral, between 6.30am and 8am.

The event village will be located at Loseby, Bowral. The first riders will return to Loseby Park from 10:00am. The last riders are expected to cross the finish line around 3:30pm.

Riders from all over Australia have registered for the event. Many will be staying in the Southern Highlands over the weekend bringing economic benefits to the local region.

An 'elite self-nominated group' of a (maximum) 75 riders will participate in the 150km Maxi Classic course, this group of riders is referred to as Highlanders.

When registering riders will be asked to provide their current Cycling Australia Race License. This is to ensure they are experienced at riding in teams or bunches.

Even though this event is not a race, this first group will be riding in an envelope with vehicles front and back for safety.

The vehicles in this group will be –

- 1 x Police vehicle (front)
- 1 x Mechanic
- 1 x Medical Vehicle (both rear). 3.3 Rouleurs Classic – 85KM

## **AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING THURSDAY 16 MAY 2024**

---

Participants will be riding on open roads with traffic. Riders must obey all traffic rules and regulations, especially Police instructions. Event vehicles from the organisation will limit the speed of the riders at some points during the ride. In this case, the riders are forbidden to overtake the organisation vehicles. Any rider who violates this rule, will be disqualified from the event, resulting in a DNF.

Please see attached Traffic Management Plan and Traffic Guidance Scheme for traffic management details.

### **CONCLUSION**

---

The Bowral Classic is a major event impacting upon an extensive part of the Shire but also promoting the area nationally and bringing in many thousands of visitors. The event organisers work closely with Council, NSW Police and Transport for NSW to minimise disruption and to follow the “Guidelines for Bicycle Road Races” and the “Guide to Traffic and Transport Management for Special Events”.

The traffic management arrangements for 2024 Bowral Classic are recommended for approval.

### **ATTACHMENTS**

---

Attachments under separate cover

1. Bowral Classic Traffic Management Plan 2024 [6.6.1 - 119 pages]
2. Bowral Classic 2024 Event Management Plan [6.6.2 - 19 pages]

---

## **6.7 Proposed Pedestrian Crossing - Lackey Road, Moss Vale**

---

**Report Author:** Traffic Engineer

**Authoriser:** Karin Targa

### **PURPOSE**

---

To review the proposal for the installation of a pedestrian crossing on Lackey Road, Moss Vale.

### **RECOMMENDATION**

---

***THAT the proposed pedestrian crossing with associated kerb blisters, signage and line marking on Lackey Road, Moss Vale be recommended for approval.***

***THAT the proposed No Stopping Zones on the approaches to the pedestrian crossing be recommended for approval.***

### **REPORT**

---

#### **BACKGROUND**

Transport for NSW plan to upgrade access to Moss Vale Railway Station. The upgrade will include a new accessible lift, paths and ramps to be installed at the location of the current pedestrian bridge which accesses Lackey Road.

A pedestrian crossing is proposed to be installed adjacent to the new lift to provide safer access for pedestrians when crossing Lackey Road.

#### **REPORT**

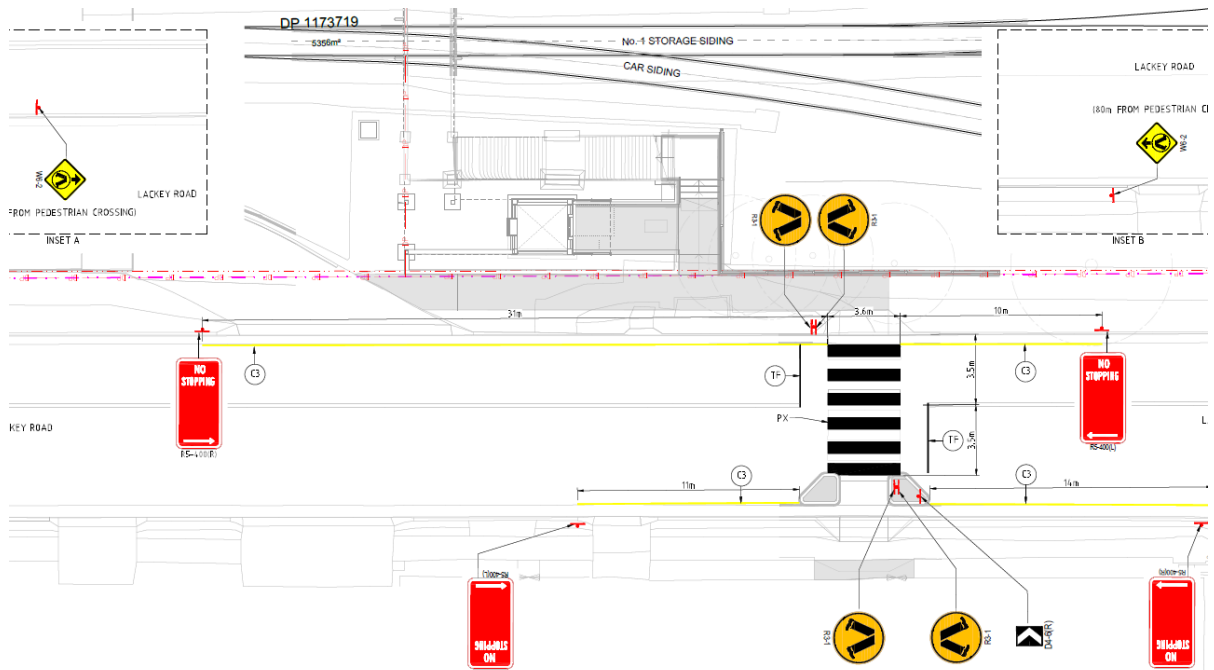
Transport for NSW proposes to improve access to Moss Vale Railway Station by upgrading the existing pedestrian bridge which connects with Lackey Road.

The upgrade works include a pedestrian lift, accessible ramps and the installation of a pedestrian crossing.

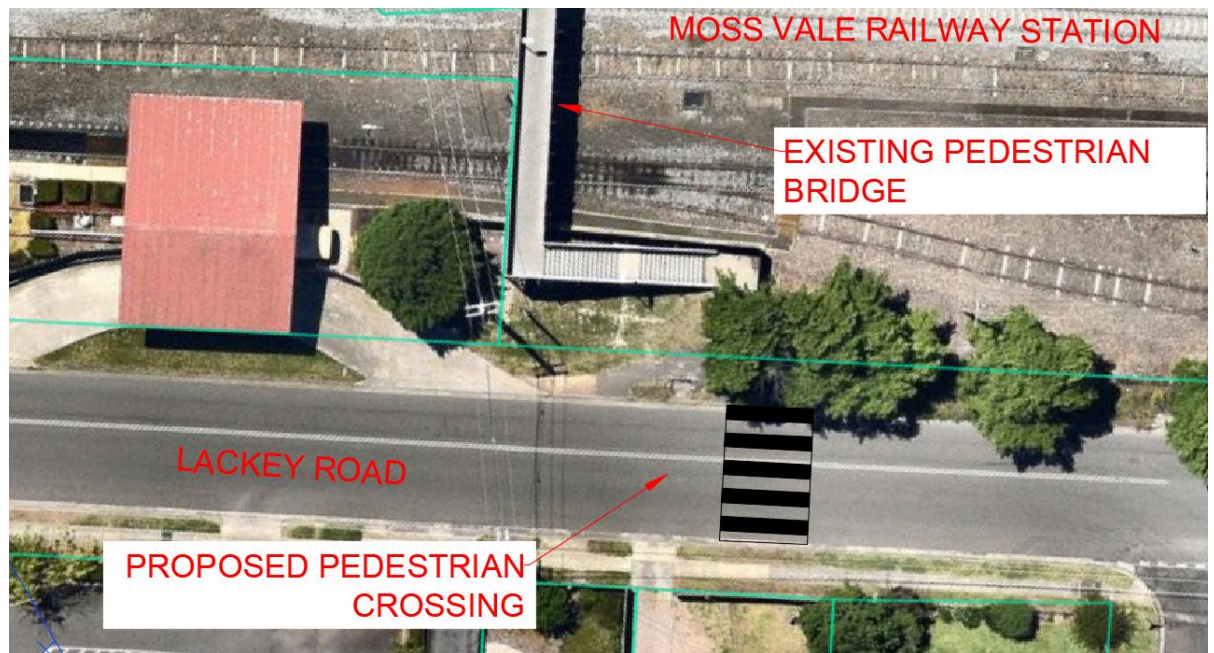
The pedestrian crossing will cross Lackey Road, allowing safer access from the existing footpath on the west side of the road to the proposed footpath and ramps on the east side of the road.

Lackey Road is a collector road with an Annual Average Daily Traffic volume of 3724 vehicles with 11.4% heavy vehicles.

**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**



**Image 1 – Proposed pedestrian crossing with signage and line marking**



**Image 2 – Aerial photograph showing existing site with proposed pedestrian crossing**



**Image 3 – Street view of the existing site showing pedestrian bridge**

The No Stopping Zones will be 31m on the southbound approach and 10m on the southbound departure of the crossing. The No Stopping Zones will be 14m on the northbound approach and 11m on the northbound departure of the crossing.

## **CONCLUSION**

---

The upgrades to Moss Vale Railway Station will provide greater accessibility to the community. The proposed pedestrian crossing will allow for safer crossing of Lackey Road to access the new pedestrian ramps and lift.













The proposed pedestrian crossing on Lackey Road, Moss Vale is recommended for approval.

## **ATTACHMENTS**

---

1. Moss Vale - Lackey Rd Pedestrian Crossing - Rev 0 - 010524 [6.7.1 - 3 pages]

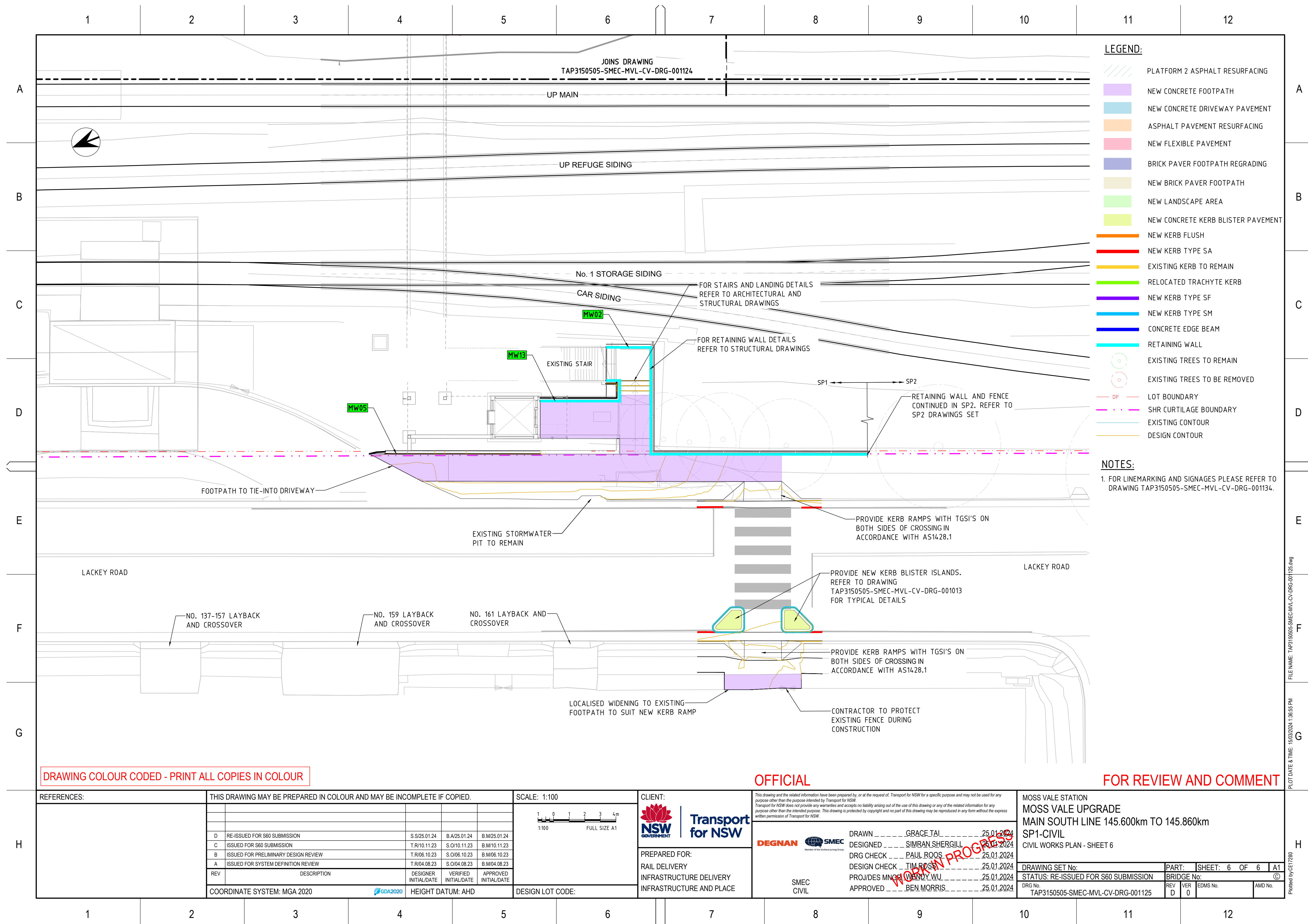


	1	2	3	4	5	6	7	8	9	10	11	12																																																		
A	<b>SIGNAGE AND LINEMARKING</b> 1. NON-FRANGIBLE SIGNS SHALL EITHER BE PLACED OUTSIDE THE CLEAR ZONE OR PROTECTED BY SAFETY BARRIER. 2. LOCATION OF PROPOSED SIGNS MAY BE ADJUSTED LONGITUDINALLY ON SITE TO SUIT SIGHT LINES SUBJECT TO APPROVAL BY THE DESIGN REPRESENTATIVE. 3. ALL SIGNAGE TO BE CLEAR OF VEGETATION AND OBSTRUCTIONS. 4. SIGNAGE AND LINEMARKING TO BE IN ACCORDANCE WITH THE DESIGN DRAWINGS, THE RELEVANT TfNSW GUIDES & STANDARDS, AUSTRALIAN STANDARDS AND THE FOLLOWING TfNSW SPECIFICATIONS:				<b>SAFETY IN DESIGN</b> 1. REFER TO THE PROJECT HAZARD LOG FOR DETAILS OF SAFETY IN DESIGN ISSUES. 2. THE CONTRACTOR IS TO COMPLETE THEIR OWN RISK ASSESSMENT OF ALL WORK PROCEDURES PRIOR TO COMMENCING WORK. 3. THE CONTRACTOR RISK ASSESSMENT IS TO INCLUDE ALL RISKS AND SHALL NOT BE LIMITED TO THOSE LISTED ABOVE.																																																									
B	- TfNSW D&C R110 - COLOURED SURFACE COATINGS FOR BUS LANES AND CYCLEWAYS - TfNSW D&C R131 - GUIDEPOSTS - TfNSW D&C R14.1 - PAVEMENT MARKING - TfNSW D&C R14.2 - RETROREFLECTIVE RAISED PAVEMENT MARKERS - TfNSW D&C R14.3 - SIGN POSTING - TfNSW D&C R14.5 - PAVEMENT MARKING - TfNSW D&C 3357 - THERMOPLASTIC ROAD MARKING MATERIAL - TfNSW D&C 3359 - PROFILE THERMOPLASTIC ROAD MARKING MATERIAL - TfNSW D&C 3360 - TWO PART COLD APPLIED ROAD MARKING MATERIAL - TfNSW D&C 3400 - MANUFACTURE AND DELIVERY OF ROAD SIGNS - TfNSW D&C B220 - PROTECTIVE TREATMENT OF BRIDGE STEEL WORK - TfNSW D&C B240 - SUPPLY OF BOLTS, NUTS, SCREWS AND WASHERS				<b>PROJECT SPECIFICATIONS</b> THE FOLLOWING PROJECT SPECIFICATIONS ARE APPLICABLE TO THE WORKS UNDER THIS PACKAGE.  TfNSW SPECIFICATIONS TS 01456.2 CONSTRUCTION SURVEYS TS 03255.2 KERBS AND CHANNELS (GUTTERS) TS 03260.2 TRENCH DRAINS TS 02158.2 EARTHWORKS TS 03264.1 CONCRETE (FOR GENERAL USE), MORTAR AND GROUT TS 03276.2 SPRAYED BITUMINOUS SURFACING (WITH CUTBACK BITUMEN) TS 03277.2 SPRAYED BITUMINOUS SURFACING (WITH POLYMER MODIFIED BITUMEN) TS 03283.2 HEAVY DUTY DENSE GRADED ASPHALT TS 03284.2 LIGHT DUTY DENSE GRADED ASPHALT TS 03315.2 GRANULAR PAVEMENT BASE AND SUBBASE MATERIALS TS 03318.2 AGGREGATES FOR ASPHALT TS 03316.2 SELECTED MATERIAL FOR FORMATION LAYERS TS 03331.2 BITUMEN ADHESION AGENT (FOR BITUMEN)																																																									
C	5. REFER TO TfNSW D&C 3400 - MANUFACTURE AND DELIVERY OF ROAD SIGNS AND AS1743 SECTION 10 FOR SIGN FACE COLOUR. 6. TO ELIMINATE EXCESSIVE GLARE FROM THE SURFACE OF A SIGN, THE SIGN SHOULD BE TURNED APPROX 5° OR 1/10 SIGN WIDTH APPROX. AWAY FROM THE NORMAL TO THE HEADLIGHT BEAM / LINE OF SIGHT 200M IN ADVANCE OF THE SIGN. THE VERTICAL AXIS OF OVERHEAD SIGNS SHOULD BE TILTED BACK 5° TO THE LINE OF SIGHT ON LEVEL AND DOWNHILL GRADES. ON UPHILL GRADES TILT BACK AT 5° TO THE LINE OF SIGHT, BUT ENSURE THAT NO REFLECTION PROBLEMS OCCUR OVER THE RANGE OF APPROACH DISTANCES.				<b>TfNSW DRAWINGS:</b> R0300 KERB AND CHANNEL SERIES R0400 EARTHWORKS SERIES  AMB T HR CI 12110 ST EARTHWORKS T HR CI 12111 SP TS 04951.2:1.0 TS 04955.1:1.0 TS 04951.1:2.0 TS 04992																																																									
D	7. LATERAL OFFSET DIMENSION FOR STRUCTURAL SIGNAGE LOCATED BEHIND GUARDRAIL SAFETY BARRIER SYSTEM HAS BEEN DEFINED BY RMS ACCEPTANCE DOCUMENT FOR SAFETY BARRIER PRODUCT. 8. CHEVRON AND DIAGONAL LAYOUT INCLUDING SIZE, ANGLE AND SPACING TO BE IN ACCORDANCE WITH THE RMS DELINEATION GUIDE. 9. RETROREFLECTIVE RAISED PAVEMENT MARKERS (RRPM'S) SHALL BE PLACED ON ALL LANE, EDGE, BARRIER LINES UNLESS NOTED OTHERWISE. SPACING OF RAISED MARKERS SHALL BE IN ACCORDANCE WITH RMS DELINEATION GUIDE AND TfNSW D&C SPECIFICATION R14.2. 10. CONNECTION OF SIGN FACES AND ASSEMBLY DETAILS TO BE UNDERTAKEN IN ACCORDANCE WITH MANUFACTURERS REQUIREMENTS AND RELEVANT TfNSW SPECIFICATIONS.																																																													
E	<b>CONCRETE NOTES</b> 1. CONCRETE EXPOSURE CLASSIFICATION FOR DRAINAGE STRUCTURES TO BE B1 UNO. 2. MINIMUM 28 DAY COMPRESSIVE STRENGTH FOR MASS CONCRETE ON SOIL SHALL BE 25MPa. 3. ALL CONCRETE WORK SHALL BE IN ACCORDANCE WITH SPECIFICATION RMS R53 UNO. 4. PRECAST CONCRETE MEMBER SHALL COMPLY WITH SPECIFICATION RMS B115. 5. MAX WATER/CEMENT RATIO SHALL BE 0.46. AND MIN TOTAL CEMENTITIOUS CONTENT SHALL BE 390kg/m <sup>3</sup> / UNO. 6. EDGES SHALL BE CHAMFERED 20x20 AND RE-ENTRANT ANGLES FILLETED 20x20 UNLESS SPECIFIED OTHERWISE. 7. NO FINES CONCRETE, 15MPa.																																																													
F	<b>DRAWING COLOUR CODED - PRINT ALL COPIES IN COLOUR</b>																																																													
G	<b>OFFICIAL FOR REVIEW AND COMMENT</b>																																																													
H	<table border="1"> <tr> <td>REFERENCES:</td> <td colspan="3">THIS DRAWING MAY BE PREPARED IN COLOUR AND MAY BE INCOMPLETE IF COPIED.</td> <td>SCALE: NTS</td> <td>CLIENT:</td> <td rowspan="2"> <small>This drawing and the related information have been prepared by, or at the request of, Transport for NSW for a specific purpose and may not be used for any purpose other than the purpose intended by Transport for NSW. Transport for NSW does not provide any warranties and accepts no liability arising out of the use of this drawing or any of the related information for any purpose other than the intended purpose. This drawing is protected by copyright and no part of this drawing may be reproduced in any form without the express written permission of Transport for NSW.</small> </td> <td colspan="6">           MOSS VALE STATION            MOSS VALE UPGRADE            MAIN SOUTH LINE 145.600km TO 145.860km            SP1-CIVIL            GENERAL NOTES - SHEET 2         </td> </tr> <tr> <td></td> <td></td> <td></td> <td></td> <td></td> <td>    <b>Transport for NSW</b> </td> <td colspan="6">           DRAWN --- GRACE TAI --- 25.01.2024            DESIGNED --- SIMRAN SHERGILL --- 25.01.2024            DRG CHECK --- PAUL ROOS --- 25.01.2024            DESIGN CHECK --- TIM ROOS --- 25.01.2024            PROJ/DES MNGR --- WENDY WU --- 25.01.2024            APPROVED --- BEN MORRIS --- 25.01.2024         </td> </tr> <tr> <td></td> <td></td> <td></td> <td></td> <td></td> <td>           PREPARED FOR:            RAIL DELIVERY            INFRASTRUCTURE DELIVERY            INFRASTRUCTURE AND PLACE         </td> <td>       <small>Member of the Austlian Joint Group</small> </td> <td colspan="6">           DRAWING SET No: _____ PART: _____ SHEET: 2 OF 2   A1            STATUS: RE-ISSUED FOR S60 SUBMISSION BRIDGE No: _____            DRG No: TAP3150505-SMEC-MVL-CV-DRG-001002 REV D VER 0 EDMS No. _____ AMD No. _____         </td> </tr> <tr> <td></td> <td colspan="3">COORDINATE SYSTEM: MGA 2020 </td> <td>HEIGHT DATUM: AHD</td> <td>DESIGN LOT CODE:</td> <td colspan="6">           SMEC CIVIL         </td> </tr> </table>												REFERENCES:	THIS DRAWING MAY BE PREPARED IN COLOUR AND MAY BE INCOMPLETE IF COPIED.			SCALE: NTS	CLIENT:	<small>This drawing and the related information have been prepared by, or at the request of, Transport for NSW for a specific purpose and may not be used for any purpose other than the purpose intended by Transport for NSW. Transport for NSW does not provide any warranties and accepts no liability arising out of the use of this drawing or any of the related information for any purpose other than the intended purpose. This drawing is protected by copyright and no part of this drawing may be reproduced in any form without the express written permission of Transport for NSW.</small>	MOSS VALE STATION MOSS VALE UPGRADE MAIN SOUTH LINE 145.600km TO 145.860km SP1-CIVIL GENERAL NOTES - SHEET 2											 <b>Transport for NSW</b>	DRAWN --- GRACE TAI --- 25.01.2024 DESIGNED --- SIMRAN SHERGILL --- 25.01.2024 DRG CHECK --- PAUL ROOS --- 25.01.2024 DESIGN CHECK --- TIM ROOS --- 25.01.2024 PROJ/DES MNGR --- WENDY WU --- 25.01.2024 APPROVED --- BEN MORRIS --- 25.01.2024											PREPARED FOR: RAIL DELIVERY INFRASTRUCTURE DELIVERY INFRASTRUCTURE AND PLACE	  <small>Member of the Austlian Joint Group</small>	DRAWING SET No: _____ PART: _____ SHEET: 2 OF 2   A1 STATUS: RE-ISSUED FOR S60 SUBMISSION BRIDGE No: _____ DRG No: TAP3150505-SMEC-MVL-CV-DRG-001002 REV D VER 0 EDMS No. _____ AMD No. _____							COORDINATE SYSTEM: MGA 2020 			HEIGHT DATUM: AHD	DESIGN LOT CODE:	SMEC CIVIL					
REFERENCES:	THIS DRAWING MAY BE PREPARED IN COLOUR AND MAY BE INCOMPLETE IF COPIED.			SCALE: NTS	CLIENT:	<small>This drawing and the related information have been prepared by, or at the request of, Transport for NSW for a specific purpose and may not be used for any purpose other than the purpose intended by Transport for NSW. Transport for NSW does not provide any warranties and accepts no liability arising out of the use of this drawing or any of the related information for any purpose other than the intended purpose. This drawing is protected by copyright and no part of this drawing may be reproduced in any form without the express written permission of Transport for NSW.</small>	MOSS VALE STATION MOSS VALE UPGRADE MAIN SOUTH LINE 145.600km TO 145.860km SP1-CIVIL GENERAL NOTES - SHEET 2																																																							
					 <b>Transport for NSW</b>		DRAWN --- GRACE TAI --- 25.01.2024 DESIGNED --- SIMRAN SHERGILL --- 25.01.2024 DRG CHECK --- PAUL ROOS --- 25.01.2024 DESIGN CHECK --- TIM ROOS --- 25.01.2024 PROJ/DES MNGR --- WENDY WU --- 25.01.2024 APPROVED --- BEN MORRIS --- 25.01.2024																																																							
					PREPARED FOR: RAIL DELIVERY INFRASTRUCTURE DELIVERY INFRASTRUCTURE AND PLACE	  <small>Member of the Austlian Joint Group</small>	DRAWING SET No: _____ PART: _____ SHEET: 2 OF 2   A1 STATUS: RE-ISSUED FOR S60 SUBMISSION BRIDGE No: _____ DRG No: TAP3150505-SMEC-MVL-CV-DRG-001002 REV D VER 0 EDMS No. _____ AMD No. _____																																																							
	COORDINATE SYSTEM: MGA 2020 			HEIGHT DATUM: AHD	DESIGN LOT CODE:	SMEC CIVIL																																																								
	1	2	3	4	5	6	7	8	9	10	11	12																																																		

FILE NAME: TAP3150505-SMEC-MVL-CV-DRG-001002.dwg

PLOT DATE & TIME: 15/03/2024 1:29:36 PM

Plotted by: JCT720



- LEGEND:**
- PLATFORM 2 ASPHALT RESURFACING
  - NEW CONCRETE FOOTPATH
  - NEW CONCRETE DRIVEWAY PAVEMENT
  - ASPHALT PAVEMENT RESURFACING
  - NEW FLEXIBLE PAVEMENT
  - BRICK PAVER FOOTPATH REGRADING
  - NEW BRICK PAVER FOOTPATH
  - NEW LANDSCAPE AREA
  - NEW CONCRETE KERB BLISTER PAVEMENT
  - NEW KERB FLUSH
  - NEW KERB TYPE SA
  - EXISTING KERB TO REMAIN
  - RELOCATED TRACHYTE KERB
  - NEW KERB TYPE SF
  - NEW KERB TYPE SM
  - CONCRETE EDGE BEAM
  - RETAINING WALL
  - EXISTING TREES TO REMAIN
  - EXISTING TREES TO BE REMOVED
  - LOT BOUNDARY
  - SHR CURTLAGE BOUNDARY
  - EXISTING CONTOUR
  - DESIGN CONTOUR

**NOTES:**  
1. FOR LINEMARKING AND SIGNAGES PLEASE REFER TO DRAWING TAP3150505-SMEC-MVL-CV-DRG-001134.

DRAWING COLOUR CODED - PRINT ALL COPIES IN COLOUR

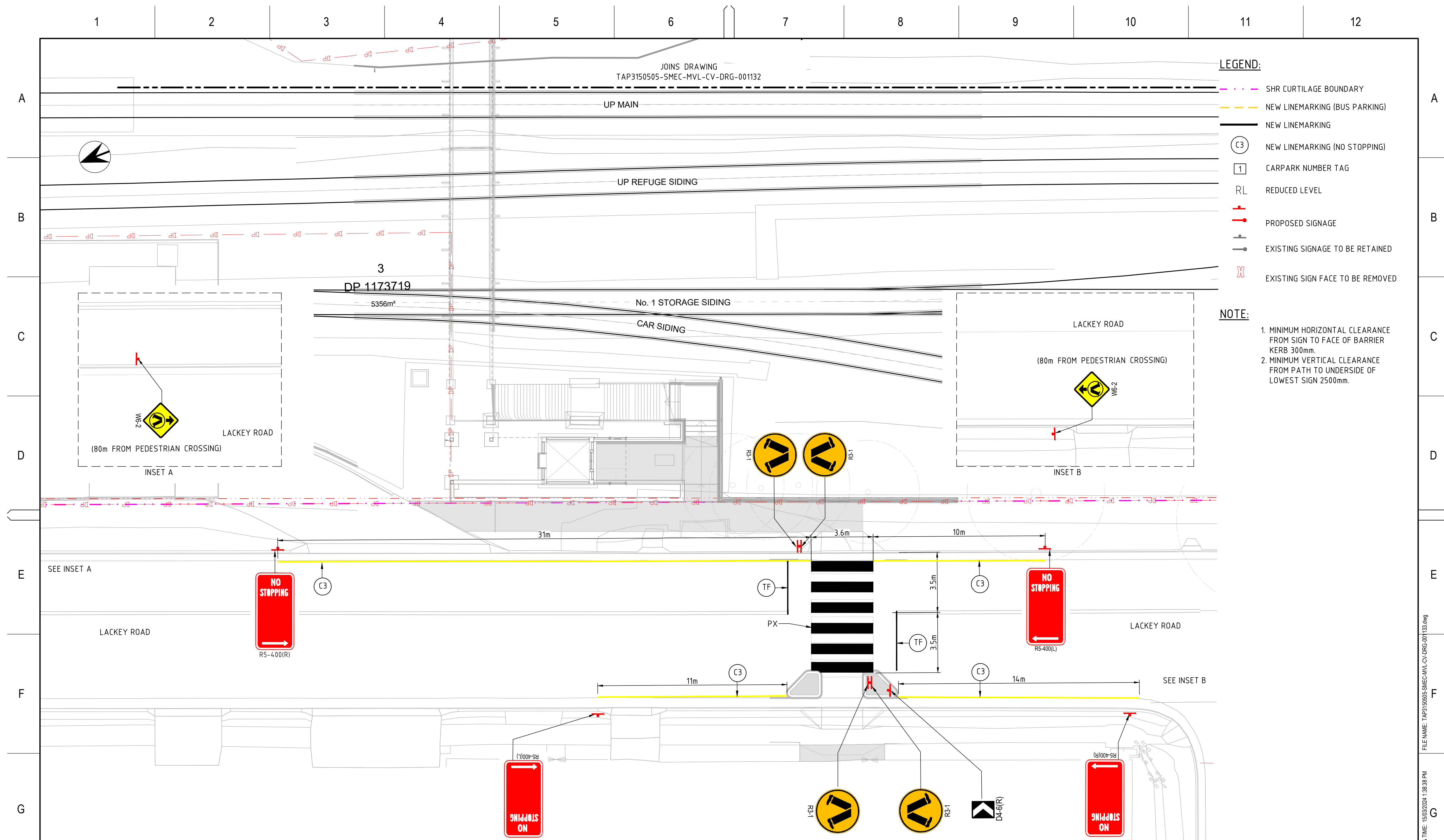
OFFICIAL

FOR REVIEW AND COMMENT

FILE NAME: TAP3150505-SMEC-MVL-CV-DRG-001125.dwg  
PLOT DATE & TIME: 15/03/2024 1:38:55 PM  
Plotted by: JCT720

REFERENCES:		THIS DRAWING MAY BE PREPARED IN COLOUR AND MAY BE INCOMPLETE IF COPIED.			SCALE: 1:100	CLIENT:	This drawing and the related information have been prepared by, or at the request of, Transport for NSW for a specific purpose and may not be used for any purpose other than the purpose intended by Transport for NSW. Transport for NSW does not provide any warranties and accepts no liability arising out of the use of this drawing or any of the related information for any purpose other than the intended purpose. This drawing is protected by copyright and no part of this drawing may be reproduced in any form without the express written permission of Transport for NSW.			MOSS VALE STATION MOSS VALE UPGRADE MAIN SOUTH LINE 145.600km TO 145.860km SP1-CIVIL CIVIL WORKS PLAN - SHEET 6																									
H		<table border="1"> <tr> <th>REV</th> <th>DESCRIPTION</th> <th>DESIGNER INITIAL/DATE</th> <th>VERIFIED INITIAL/DATE</th> <th>APPROVED INITIAL/DATE</th> </tr> <tr> <td>D</td> <td>RE-ISSUED FOR S60 SUBMISSION</td> <td>S.S/25.01.24</td> <td>B.A/25.01.24</td> <td>B.M/25.01.24</td> </tr> <tr> <td>C</td> <td>ISSUED FOR S60 SUBMISSION</td> <td>T.R/10.11.23</td> <td>S.O/10.11.23</td> <td>B.M/10.11.23</td> </tr> <tr> <td>B</td> <td>ISSUED FOR PRELIMINARY DESIGN REVIEW</td> <td>T.R/06.10.23</td> <td>S.O/06.10.23</td> <td>B.M/06.10.23</td> </tr> <tr> <td>A</td> <td>ISSUED FOR SYSTEM DEFINITION REVIEW</td> <td>T.R/04.08.23</td> <td>S.O/04.08.23</td> <td>B.M/04.08.23</td> </tr> </table>			REV	DESCRIPTION	DESIGNER INITIAL/DATE	VERIFIED INITIAL/DATE	APPROVED INITIAL/DATE	D	RE-ISSUED FOR S60 SUBMISSION	S.S/25.01.24	B.A/25.01.24	B.M/25.01.24	C	ISSUED FOR S60 SUBMISSION	T.R/10.11.23	S.O/10.11.23	B.M/10.11.23	B	ISSUED FOR PRELIMINARY DESIGN REVIEW	T.R/06.10.23	S.O/06.10.23	B.M/06.10.23	A	ISSUED FOR SYSTEM DEFINITION REVIEW	T.R/04.08.23	S.O/04.08.23	B.M/04.08.23		 	 	DRAWN: GRACE TAI 25.01.2024 DESIGNED: SIMRAN SHERGILL 25.01.2024 DRG CHECK: PAUL ROOS 25.01.2024 DESIGN CHECK: TIM ROSS 25.01.2024 PROJ/DES MGR: WENDY WU 25.01.2024 APPROVED: BEN MORRIS 25.01.2024	DRAWING SET No: STATUS: RE-ISSUED FOR S60 SUBMISSION DRG No: TAP3150505-SMEC-MVL-CV-DRG-001125	PART: SHEET: 6 OF 6 A1 BRIDGE No: REV: D IVER: 0 EDMS No: AMD No:
REV	DESCRIPTION	DESIGNER INITIAL/DATE	VERIFIED INITIAL/DATE	APPROVED INITIAL/DATE																															
D	RE-ISSUED FOR S60 SUBMISSION	S.S/25.01.24	B.A/25.01.24	B.M/25.01.24																															
C	ISSUED FOR S60 SUBMISSION	T.R/10.11.23	S.O/10.11.23	B.M/10.11.23																															
B	ISSUED FOR PRELIMINARY DESIGN REVIEW	T.R/06.10.23	S.O/06.10.23	B.M/06.10.23																															
A	ISSUED FOR SYSTEM DEFINITION REVIEW	T.R/04.08.23	S.O/04.08.23	B.M/04.08.23																															
		COORDINATE SYSTEM: MGA 2020	HEIGHT DATUM: AHD	DESIGN LOT CODE:	SMEC CIVIL																														





**LEGEND:**

- SHR CURTLAGE BOUNDARY
- NEW LINEMARKING (BUS PARKING)
- NEW LINEMARKING
- NEW LINEMARKING (NO STOPPING)
- CARPARK NUMBER TAG
- REDUCED LEVEL
- PROPOSED SIGNAGE
- EXISTING SIGNAGE TO BE RETAINED
- EXISTING SIGN FACE TO BE REMOVED

**NOTE:**

1. MINIMUM HORIZONTAL CLEARANCE FROM SIGN TO FACE OF BARRIER KERB 300mm.
2. MINIMUM VERTICAL CLEARANCE FROM PATH TO UNDERSIDE OF LOWEST SIGN 2500mm.

DRAWING COLOUR CODED - PRINT ALL COPIES IN COLOUR

OFFICIAL

FOR REVIEW AND COMMENT

FILE NAME: TAP3150505-SMEC-MVL-CV-DRG-001133.dwg

PLOT DATE & TIME: 15/03/2024 1:39:39 PM

Plotted by: JCE/2720

REFERENCES:		THIS DRAWING MAY BE PREPARED IN COLOUR AND MAY BE INCOMPLETE IF COPIED.			SCALE: 1:100	CLIENT:	<small>This drawing and the related information have been prepared by, or at the request of, Transport for NSW for a specific purpose and may not be used for any purpose other than the purpose intended by Transport for NSW. Transport for NSW does not provide any warranties and accepts no liability arising out of the use of this drawing or any of the related information for any purpose other than the intended purpose. This drawing is protected by copyright and no part of this drawing may be reproduced in any form without the express written permission of Transport for NSW.</small>	MOSS VALE STATION MOSS VALE UPGRADE MAIN SOUTH LINE 145.600km TO 145.860km SP1-CIVIL LINEMARKING AND SIGNAGE PLAN - SHEET 4																						
<table border="1"> <tr> <th>REV</th> <th>DESCRIPTION</th> <th>DESIGNER INITIAL/DATE</th> <th>VERIFIED INITIAL/DATE</th> <th>APPROVED INITIAL/DATE</th> </tr> <tr> <td>C</td> <td>RE-ISSUED FOR S60 SUBMISSION</td> <td>S.S/25.01.24</td> <td>B.A/25.01.24</td> <td>B.M/25.01.24</td> </tr> <tr> <td>B</td> <td>ISSUED FOR S60 SUBMISSION</td> <td>T.R/10.11.23</td> <td>S.O/10.11.23</td> <td>B.M/10.11.23</td> </tr> <tr> <td>A</td> <td>ISSUED FOR PRELIMINARY DESIGN REVIEW</td> <td>T.R/06.10.23</td> <td>S.O/06.10.23</td> <td>B.M/06.10.23</td> </tr> </table>		REV	DESCRIPTION	DESIGNER INITIAL/DATE	VERIFIED INITIAL/DATE	APPROVED INITIAL/DATE		C	RE-ISSUED FOR S60 SUBMISSION	S.S/25.01.24	B.A/25.01.24	B.M/25.01.24	B	ISSUED FOR S60 SUBMISSION	T.R/10.11.23	S.O/10.11.23	B.M/10.11.23	A	ISSUED FOR PRELIMINARY DESIGN REVIEW	T.R/06.10.23	S.O/06.10.23	B.M/06.10.23	COORDINATE SYSTEM: MGA 2020			DESIGN LOT CODE:			DRAWN: GRACE TAI 25.01.2024 DESIGNED: SIMRAN SHERGILL 25.01.2024 DRG CHECK: PAUL ROOS 25.01.2024 DESIGN CHECK: TIM ROSS 25.01.2024 PROJ/DES MNGR: WENDY WU 25.01.2024 APPROVED: BEN MORRIS 25.01.2024	DRAWING SET No: _____ STATUS: RE-ISSUED FOR S60 SUBMISSION DRG No: TAP3150505-SMEC-MVL-CV-DRG-001133
REV	DESCRIPTION	DESIGNER INITIAL/DATE	VERIFIED INITIAL/DATE	APPROVED INITIAL/DATE																										
C	RE-ISSUED FOR S60 SUBMISSION	S.S/25.01.24	B.A/25.01.24	B.M/25.01.24																										
B	ISSUED FOR S60 SUBMISSION	T.R/10.11.23	S.O/10.11.23	B.M/10.11.23																										
A	ISSUED FOR PRELIMINARY DESIGN REVIEW	T.R/06.10.23	S.O/06.10.23	B.M/06.10.23																										

---

## **6.8 Railway Avenue, Wingello - Removal of Pedestrian Refuge**

---

**Report Author: Traffic Engineer**

**Authoriser: Karin Targa**

### **PURPOSE**

---

To not support a pedestrian refuge from previously approved road work design plan in relation to Casburn Park, Wingello upgrade works

### **RECOMMENDATION**

---

***THAT the pedestrian refuge shown in the design plans produced by Complete, design plan No. 3393, in relation the Casburn Park upgrade, not be recommended for construction.***

### **REPORT**

---

#### **BACKGROUND**

At the 24 August 2023 Local Traffic Committee meeting a civil works design plan in relation to upgrade works to Casburn Park was recommended for approval.

The civil works included sealing the verge, kerb and gutter, footpaths and a pedestrian refuge.

#### **REPORT**

The design plans for the civil works in relation to Casburn Park upgrade work was recommended for approval by Traffic Committee at the 24 August 2023 meeting.

The civil design plans included a pedestrian refuge. Dimensions of the refuge and west bound lane were shown to be adequate.

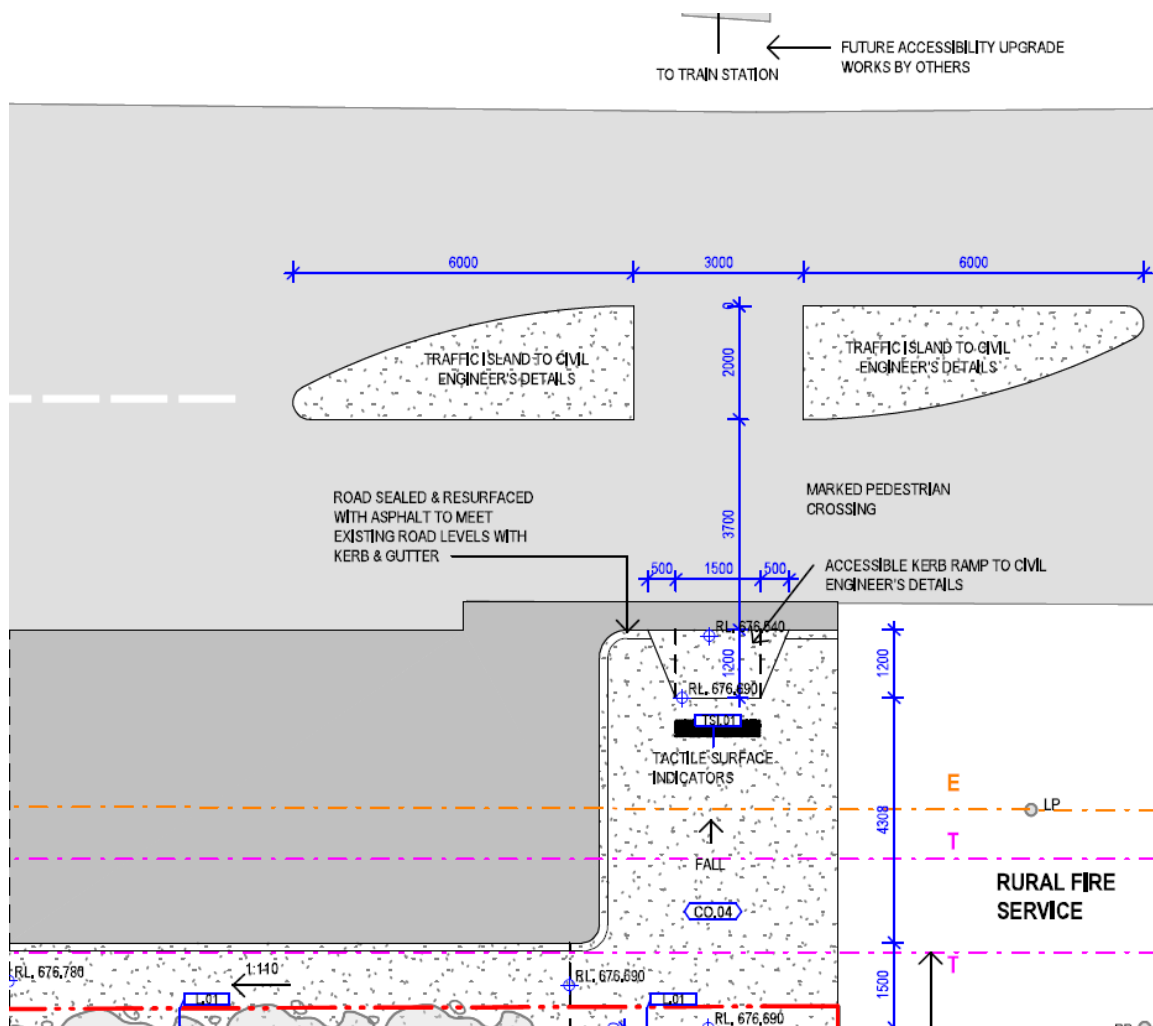
Construction of the civil works began in February 2024. It was noticed that the overall final road width was 8m. A 2m refuge would leave only 3m for each travel lane. The civil works included a concrete dish drain at the interface between the road and the parking area. The 3m lane width on the west bound lane is further reduced in width due to the edge of the dish drain.

The 3m width of the lanes will force vehicles to the edge of the lanes where they will drive on the dish gutter on the west bound lane, and on the broken edges of the road on the east bound lane or be forced to drive close to the refuge and waiting pedestrians.

The Annual Average Daily Traffic volume for Railway Parade is 839 vehicles with 10% heavy vehicles.



**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**



**Image 1 – Civil design plan showing the proposed pedestrian refuge**

This report proposes to not include the pedestrian refuge due to the insufficient road width.

A footpath and kerb ramp are provided at the desired crossing point to the train station. There is over 500m of sight distance when looking to the east. There is approximately 180m of sight distance when looking to the west which includes a stop sign that will slow traffic. The speed limit on the portion of Railway Parade is 50km/h.

**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**

---



**Image 2 – Aerial photo with subject location shown with the red dot**



**Image 3 – Pre-construction image of the subject location (looking east)**





**Image 4 – Civil works during construction showing the dish drain (looking east)**

## **CONCLUSION**

---

The road width on Railway Parade, Wingello, is not enough to install a pedestrian refuge and still have sufficient road width, particularly for heavy vehicles.

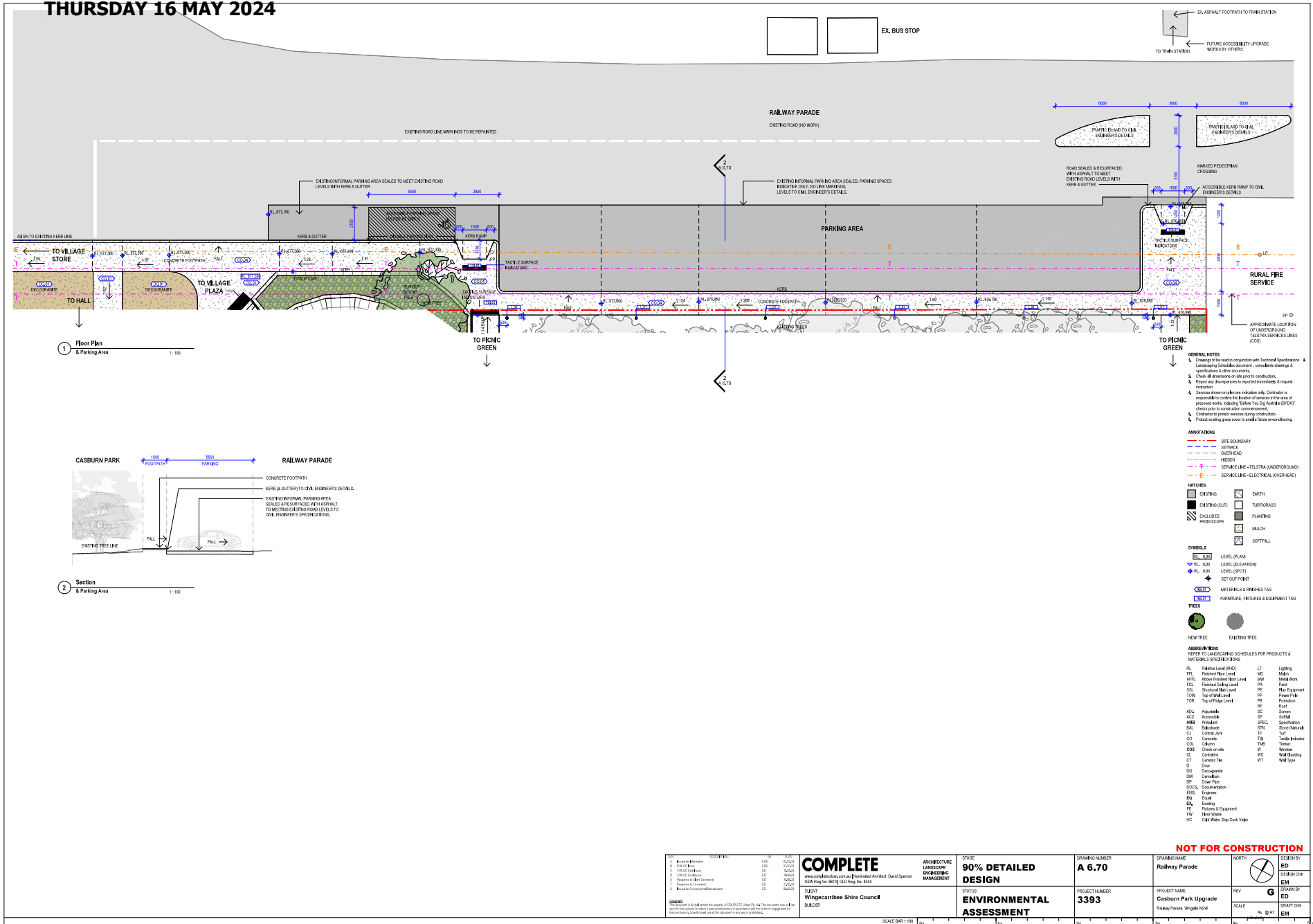
It is not recommended that the pedestrian refuge as shown in the civil design plans for the Casburn Park upgrade be constructed.

## **ATTACHMENTS**

---

1. Casburn Park Upgrade - Parking and Pedestrian Facilities [6.8.1 - 1 page]

# AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING THURSDAY 16 MAY 2024





---

## **6.9 Access Road Arrangements for 6 Lot Subdivision at 267 Bundanoon Road Exeter**

---

**Report Author: Traffic Engineer**

**Authoriser: Karin Targa**

### **PURPOSE**

---

To assess the proposed access road arrangements for a 6 lot subdivision at 267 Bundanoon Road, Exeter

### **RECOMMENDATION**

---

***THAT the median island, signage and line marking arrangements as shown in design plan drawing No. 22071-D01-V8 designed by CJP Consulting Engineers for the access to the proposed subdivision be recommended for approval.***

***THAT further information be requested for the proposed turning area on Ringwood Lane, Exeter***

### **REPORT**

---

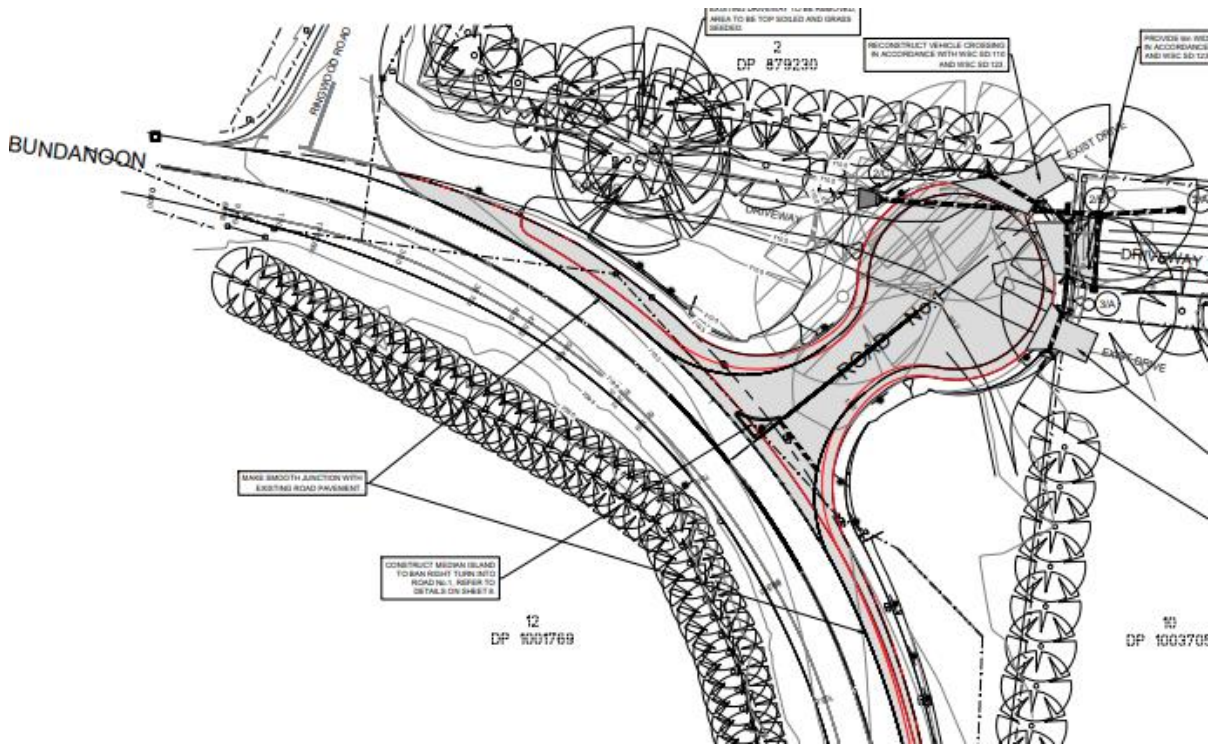
Council received a development application for a 6 lot subdivision at 267 Bundanoon Road, Exeter (DA No. 22/0890).

The subdivision requires an access road which is proposed to be a 7m radius cul-de-sac accessing from Bundanoon Road. The cul-de-sac will be located approximately 70m east of Ringwood Road.

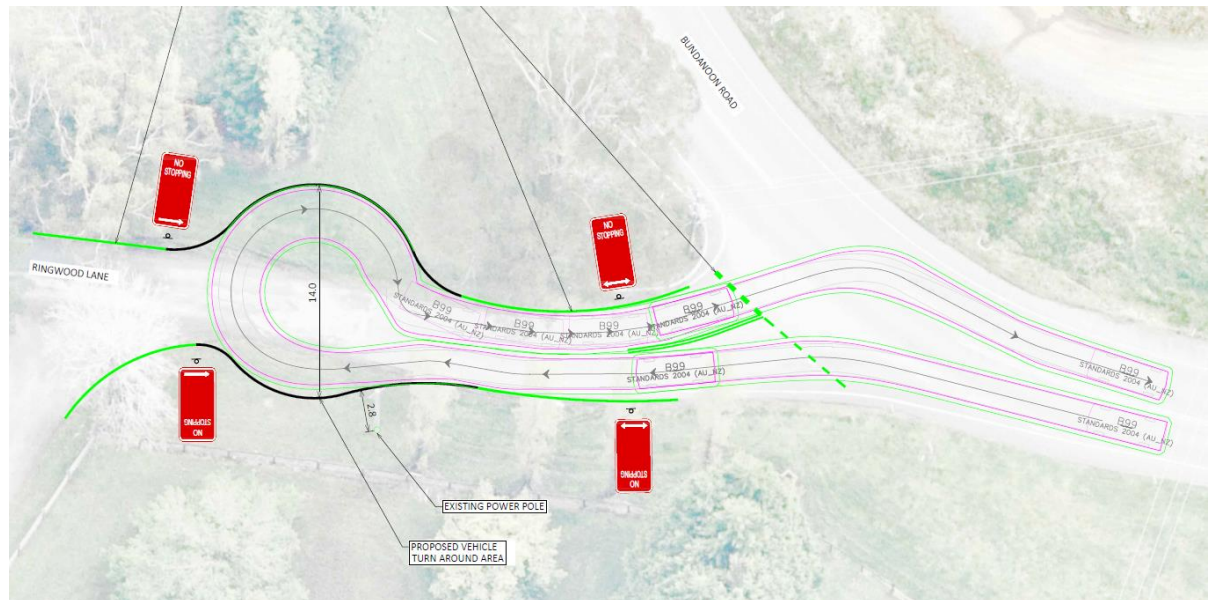
Due to limited sight distance the entrance is proposed to not allow right turns into the access road. Signage and a median island will be in place to notify drivers of the No Right Turn and restrict the right turn movement.

A 7m radius turning area is proposed to be constructed on Ringwood Lane to cater for the inability to turn right into the subdivision entry road. Ringwood Lane is located 170m west of the proposed access road to the subdivision.

**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**



**Image 1 – Proposed access road from Bundanoon Road**

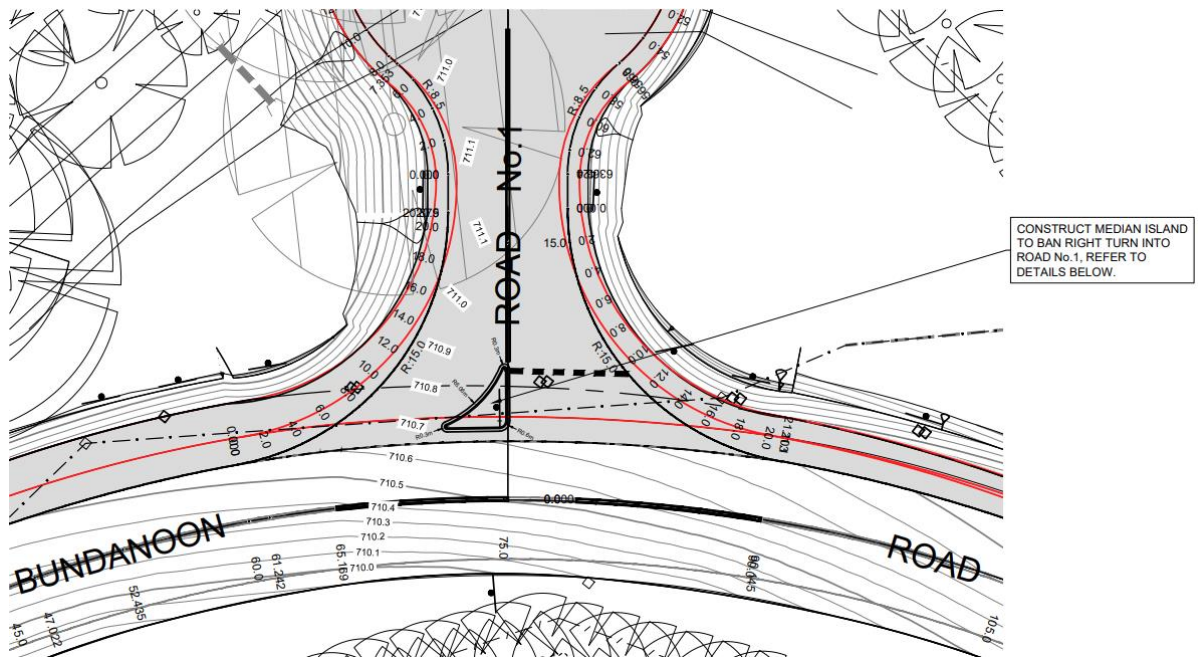


**Image 2 – Proposed turning area on Ringwood Lane, Exeter**



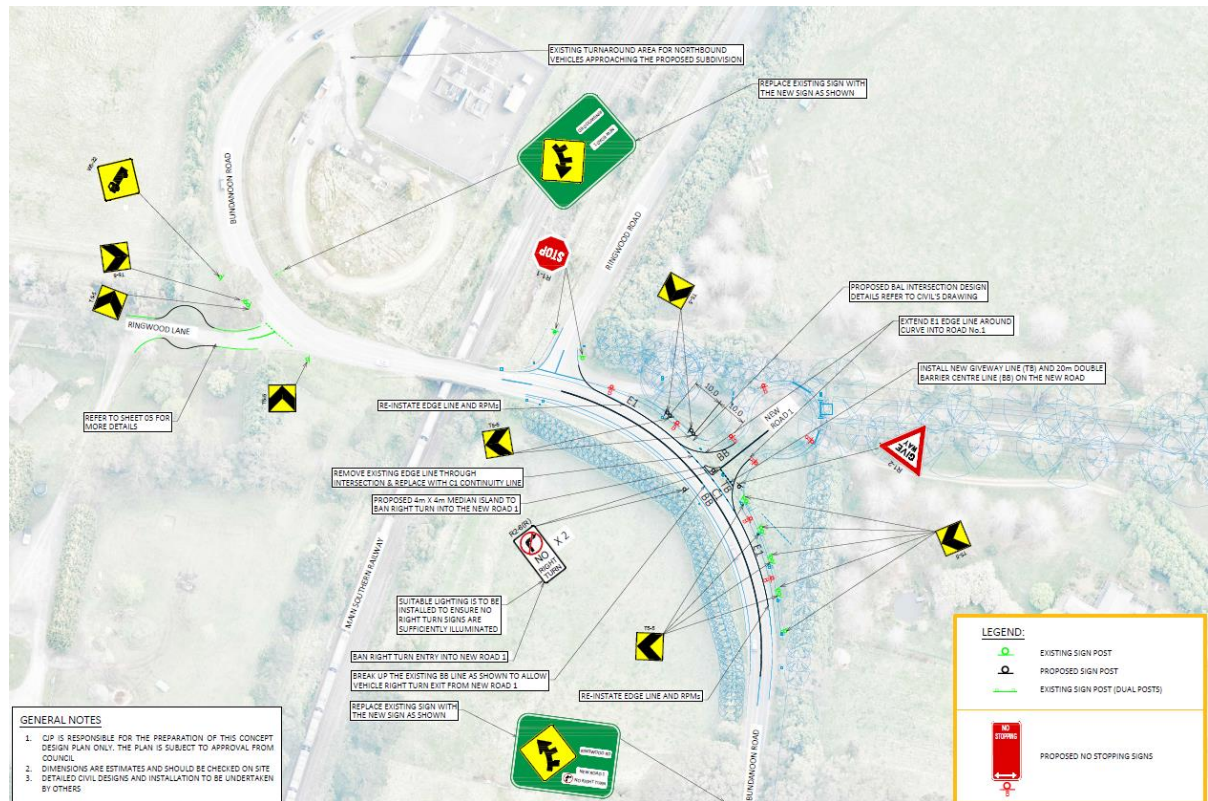


**Image 3 – Aerial image of the subject area with location of proposed access road at the red dot**



**Image 4 – Proposed median island to prevent right turn movements into the subdivision**

# AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING THURSDAY 16 MAY 2024



**Image 5 – Signage and line marking plan**

Complete signage and line marking plan, swept turning paths and road safety audit are attached with this report.

Council engaged the services of an expert traffic engineer to review the design plans. Sight distances were checked, and measures taken to address sight distance deficiencies

The signage proposed includes Give Way, Curve Alignment Makers, No Right Turn, No Stopping and advanced warning of the road layout. Double barrier lines, continuity lines and edge lines are proposed. The arrangement of the signage and line marking are attached.

## CONCLUSION

Signage and line marking, sight distances, and road dimensions have been assessed to be appropriate.

The proposed access road for the 6 lot subdivision at 267 Bundanoon Road is to be recommended for approval.

The plans provided for the proposed turning area on Ringwood Lane does not have enough information at this stage to be recommended for approval. More information on signage, line marking, right of way of vehicles, and engineering specifications, such as retaining walls, batters, road material and drainage will need to be provided.



**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**

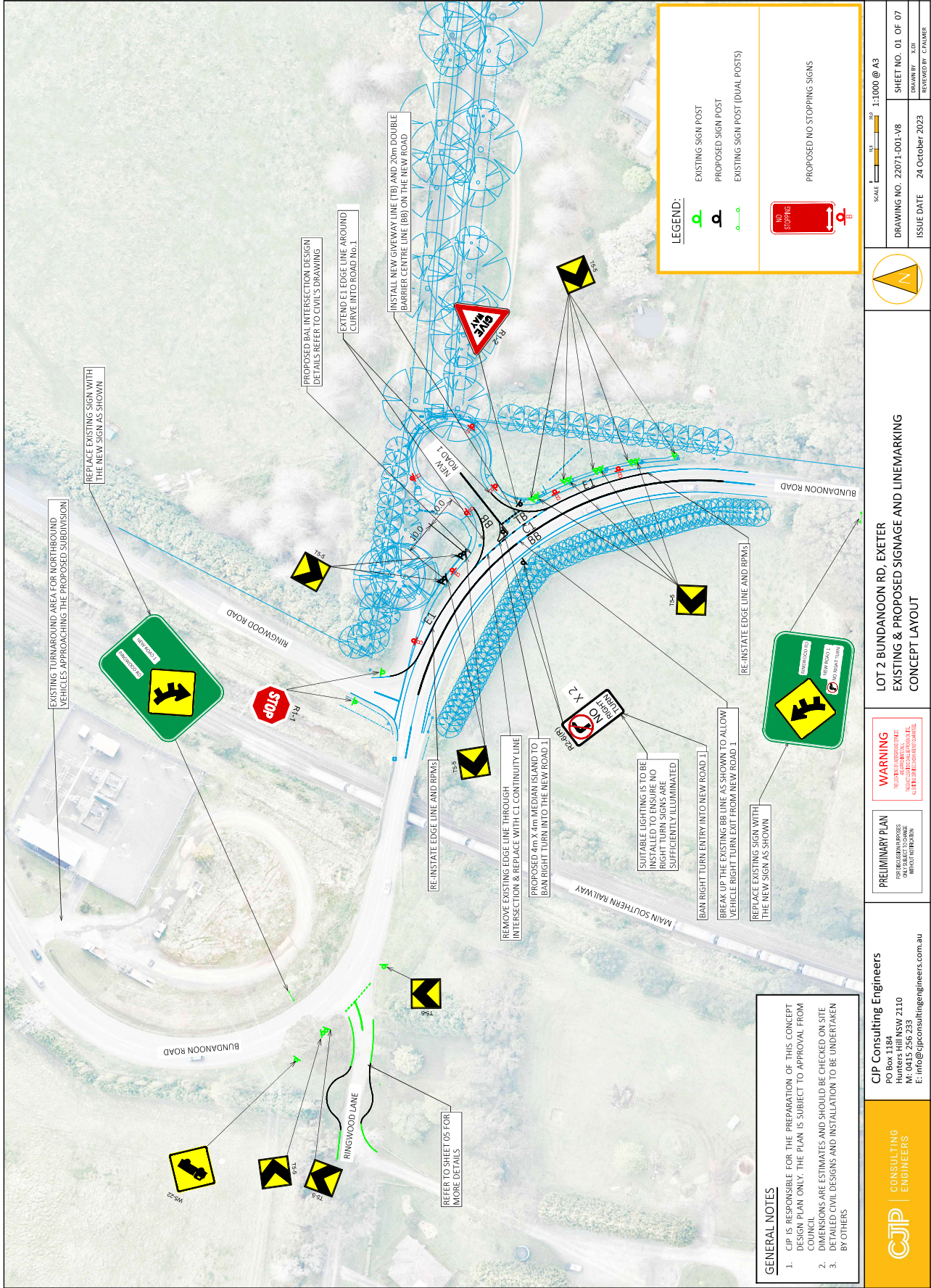
---

**ATTACHMENTS**

---

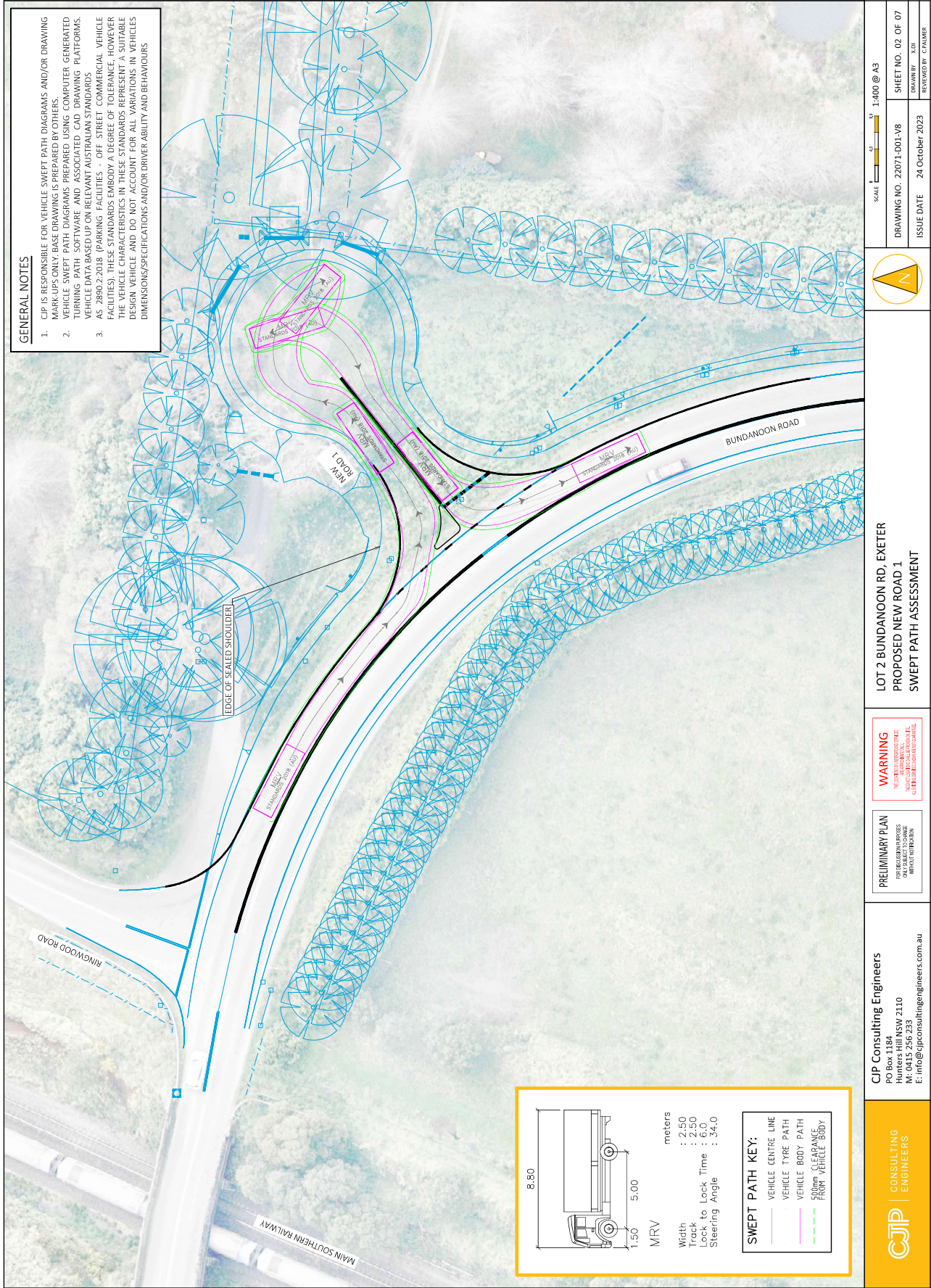
1. 267 Bundanoon Road Signage, Line Marking and Swept Paths [6.9.1 - 7 pages]
2. 267 Bundanoon Road Road Safety Audit [6.9.2 - 13 pages]

# AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING THURSDAY 16 MAY 2024



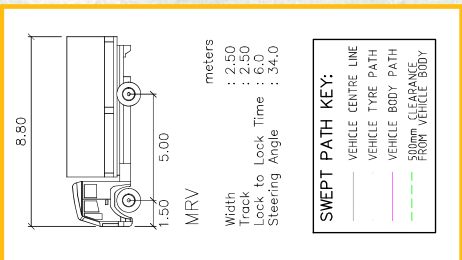


# AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING THURSDAY 16 MAY 2024



**GENERAL NOTES**

1. CIP IS RESPONSIBLE FOR VEHICLE SWEEP PATH DIAGRAMS AND/OR DRAWING MARK-UPS ONLY. BASE DRAWING IS PREPARED BY OTHERS.
2. VEHICLE SWEEP PATH DIAGRAMS PREPARED USING COMPUTER GENERATED TURNING PATH SOFTWARE AND ASSOCIATED CAD DRAWING PLATFORMS. VEHICLE DATA BASED UP ON RELEVANT AUSTRALIAN STANDARDS.
3. AS 2890.2:2018 (PARKING FACILITIES - OFF STREET COMMERCIAL VEHICLE FACILITIES). THESE STANDARDS EMBODY A DEGREE OF TOLERANCE, HOWEVER THE VEHICLE CHARACTERISTICS IN THESE STANDARDS REPRESENT A SUITABLE DESIGN VEHICLE AND DO NOT ACCOUNT FOR ALL VARIATIONS IN VEHICLES DIMENSIONS/SPECIFICATIONS AND/OR DRIVER ABILITY AND BEHAVIOURS.



**GENERAL NOTES**

1. CIP IS RESPONSIBLE FOR VEHICLE SWEEP PATH DIAGRAMS AND/OR DRAWING MARK-UPS ONLY. BASE DRAWING IS PREPARED BY OTHERS.
2. VEHICLE SWEEP PATH DIAGRAMS PREPARED USING COMPUTER GENERATED TURNING PATH SOFTWARE AND ASSOCIATED CAD DRAWING PLATFORMS. VEHICLE DATA BASED UP ON RELEVANT AUSTRALIAN STANDARDS.
3. AS 2890.2:2018 (PARKING FACILITIES - OFF STREET COMMERCIAL VEHICLE FACILITIES). THESE STANDARDS EMBODY A DEGREE OF TOLERANCE, HOWEVER THE VEHICLE CHARACTERISTICS IN THESE STANDARDS REPRESENT A SUITABLE DESIGN VEHICLE AND DO NOT ACCOUNT FOR ALL VARIATIONS IN VEHICLES DIMENSIONS/SPECIFICATIONS AND/OR DRIVER ABILITY AND BEHAVIOURS.

SCALE 0 5 10 METERS  
 SCALE 1:400 @ A3  
 DRAWING NO. 22071-001-V8  
 SHEET NO. 02 OF 07  
 ISSUE DATE 24 October 2023  
 DRAWN BY C.A.D.  
 REVIEWED BY C.FALMER



LOT 2 BUNDANOON RD, EXETER  
 PROPOSED NEW ROAD 1  
 SWEEP PATH ASSESSMENT

**WARNING**  
 THIS IS A PRELIMINARY PLAN  
 FOR INFORMATION PURPOSES  
 ONLY. IT IS NOT TO BE USED FOR  
 ANY OTHER PURPOSES WITHOUT  
 THE ENGINEER'S CONSENT.

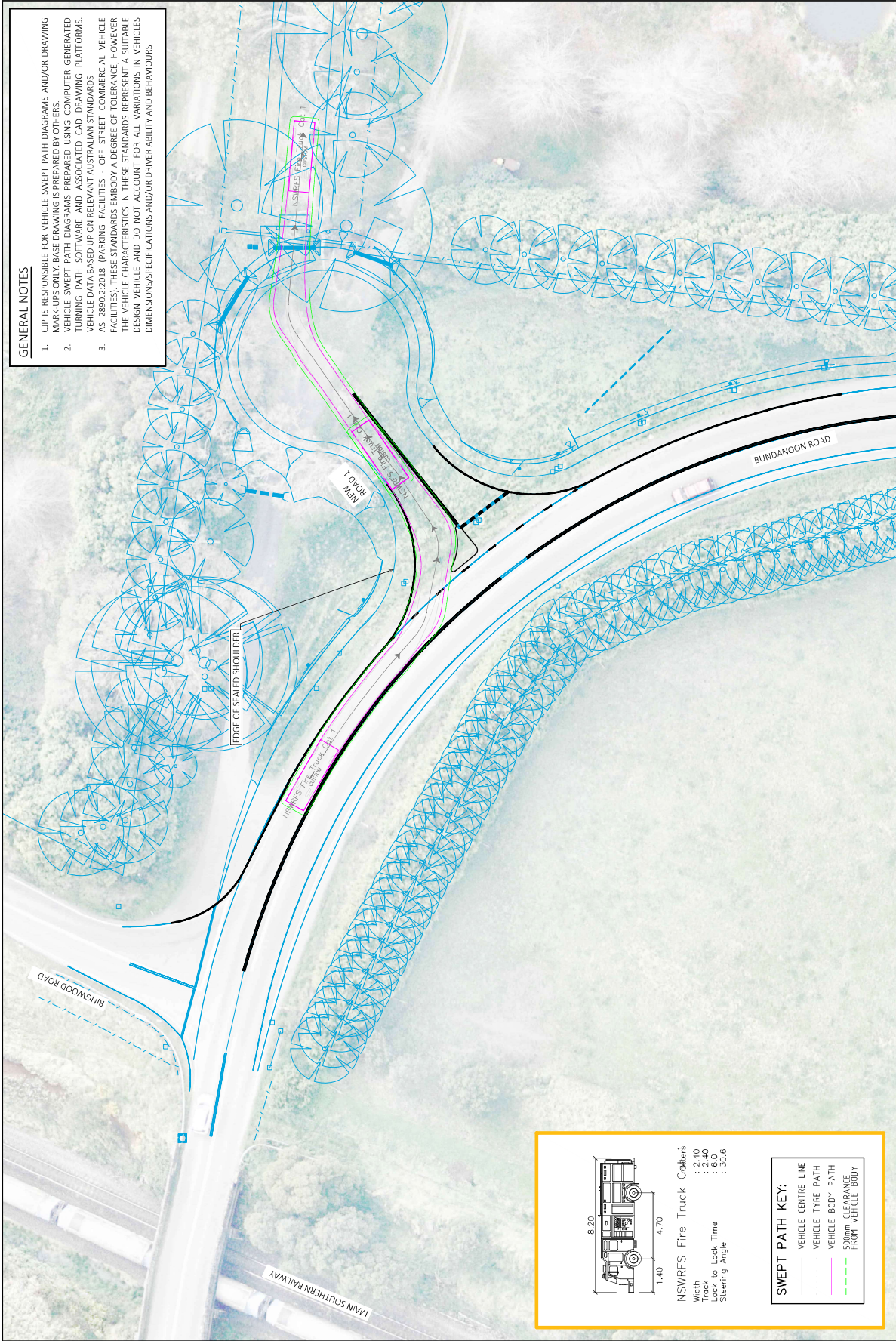
**PRELIMINARY PLAN**  
 FOR INFORMATION PURPOSES  
 ONLY. IT IS NOT TO BE USED  
 FOR ANY OTHER PURPOSES  
 WITHOUT THE ENGINEER'S  
 CONSENT.

**CJP Consulting Engineers**  
 PO Box 1184  
 Hunters Hill NSW 2110  
 M: 0415 256 233  
 E: info@cjpconsultingengineers.com.au





# AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING THURSDAY 16 MAY 2024



**GENERAL NOTES**

1. CIP IS RESPONSIBLE FOR VEHICLE SWEEP PATH DIAGRAMS AND/OR DRAWING MARK-UPS ONLY. BASE DRAWING IS PREPARED BY OTHERS.
2. VEHICLE SWEEP PATH DIAGRAMS PREPARED USING COMPUTER GENERATED TURNING PATH SOFTWARE AND ASSOCIATED CAD DRAWING PLATFORMS, VEHICLE DATA BASED UP ON RELEVANT AUSTRALIAN STANDARDS AS 2890.2:2018 (PARKING FACILITIES - OFF STREET COMMERCIAL VEHICLE FACILITIES). THESE STANDARDS EMBODY A DEGREE OF TOLERANCE, HOWEVER THE VEHICLE CHARACTERISTICS IN THESE STANDARDS REPRESENT A SUITABLE DESIGN VEHICLE AND DO NOT ACCOUNT FOR ALL VARIATIONS IN VEHICLES DIMENSIONS/SPECIFICATIONS AND/OR DRIVER ABILITY AND BEHAVIOURS
- 3.

**NSWRFS Fire Truck** *Coherent*

Width : 2.40  
Track : 2.40  
Lock to Lock Time : 6.0  
Steering Angle : 30.6

**SWEPT PATH KEY:**

- VEHICLE CENTRE LINE
- VEHICLE TYRE PATH
- VEHICLE BODY PATH
- 300mm CLEARANCE FROM VEHICLE BODY

Prepared by Administrator

SCALE	1:400 @ A3
DRAWING NO.	22071-001-V8
SHEET NO.	03 OF 07
ISSUE DATE	24 October 2023
DRAWN BY	CAD
REVIEWED BY	CPALMER



**LOT 2 BUNDANOON RD, EXETER  
PROPOSED NEW ROAD 1  
SWEEP PATH ASSESSMENT**

WARNING

THIS IS A PRELIMINARY PLAN  
FOR INFORMATION PURPOSES  
ONLY. IT IS NOT TO BE USED FOR  
CONSTRUCTION OR CONTRACTS.

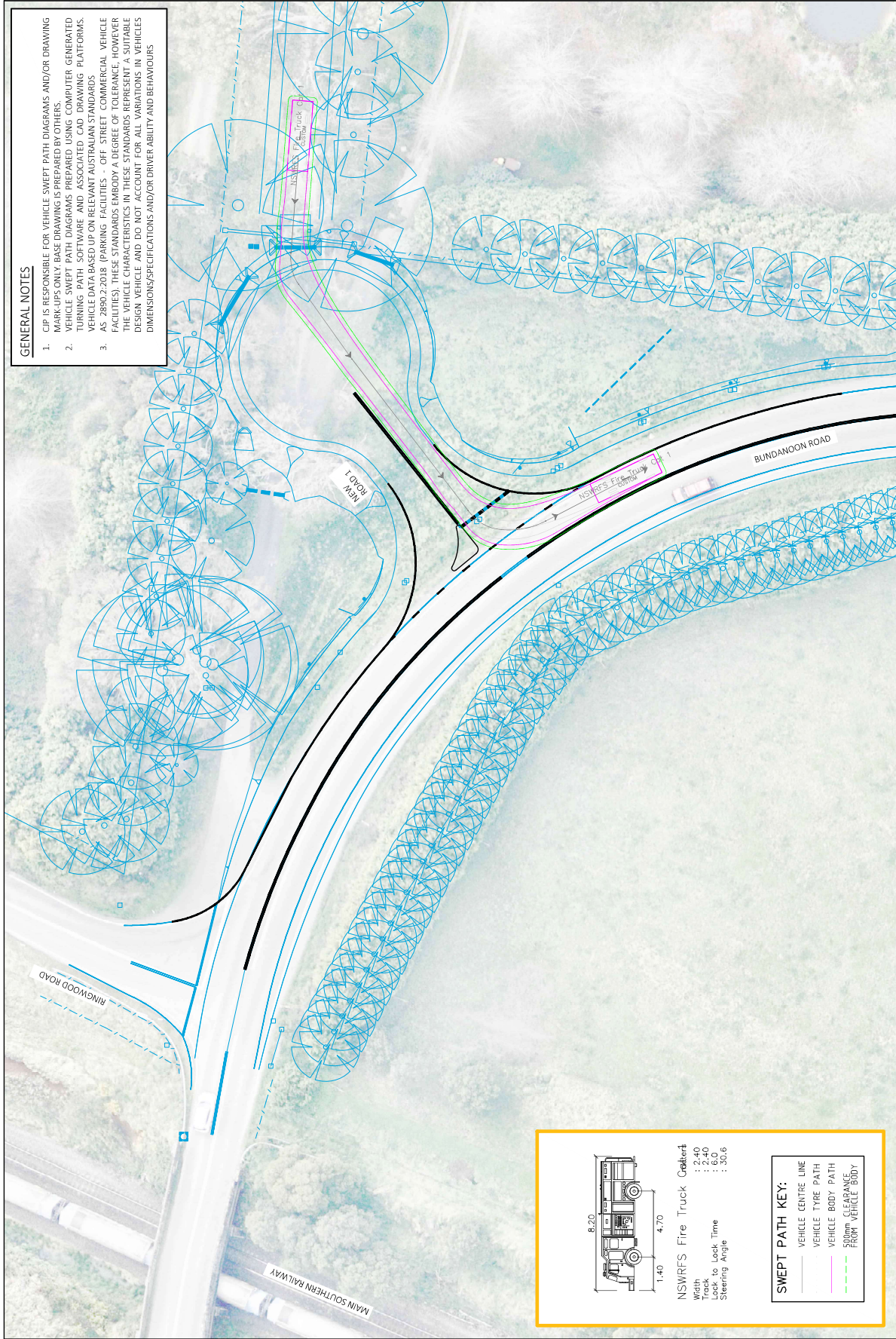
**PRELIMINARY PLAN**  
FOR INFORMATION PURPOSES  
ONLY. IT IS NOT TO BE USED  
FOR CONSTRUCTION.

**CJP Consulting Engineers**  
PO Box 1184  
Hunters Hill NSW 2110  
M: 0415 256 233  
E: info@cjpconsultingengineers.com.au





# AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING THURSDAY 16 MAY 2024



**GENERAL NOTES**

1. CIP IS RESPONSIBLE FOR VEHICLE SWEEP PATH DIAGRAMS AND/OR DRAWING MARK-UPS ONLY. BASE DRAWING IS PREPARED BY OTHERS.
2. VEHICLE SWEEP PATH DIAGRAMS PREPARED USING COMPUTER GENERATED TURNING PATH SOFTWARE AND ASSOCIATED CAD DRAWING PLATFORMS, VEHICLE DATA BASED UP ON RELEVANT AUSTRALIAN STANDARDS.
3. AS 2890.2:2018 (PARKING FACILITIES - OFF STREET COMMERCIAL VEHICLE FACILITIES). THESE STANDARDS EMBODY A DEGREE OF TOLERANCE, HOWEVER THE VEHICLE CHARACTERISTICS IN THESE STANDARDS REPRESENT A SUITABLE DESIGN VEHICLE AND DO NOT ACCOUNT FOR ALL VARIATIONS IN VEHICLES DIMENSIONS/SPECIFICATIONS AND/OR DRIVER ABILITY AND BEHAVIOURS.

**NSWRTS Fire Truck** *Coherent*

Width : 2.40  
Track : 2.40  
Lock to Lock Time : 6.0  
Steering Angle : 30.6

**SWEPT PATH KEY:**

- VEHICLE CENTRE LINE
- VEHICLE TYRE PATH
- VEHICLE BODY PATH
- 300mm CLEARANCE FROM VEHICLE BODY

**CJP Consulting Engineers**  
PO Box 1184  
Hunters Hill NSW 2110  
M: 0415 256 233  
E: info@cjpconsultingengineers.com.au

**WARNING**  
THIS IS A PRELIMINARY PLAN  
FOR INFORMATION PURPOSES  
ONLY. IT IS NOT TO BE USED FOR  
CONSTRUCTION PURPOSES.

**PRELIMINARY PLAN**  
FOR INFORMATION PURPOSES  
ONLY. IT IS NOT TO BE USED FOR  
CONSTRUCTION PURPOSES.

**LOT 2 BUNDANOON RD, EXETER  
PROPOSED NEW ROAD 1  
SWEEP PATH ASSESSMENT**

SCALE 1:400 @ A3

DRAWING NO. 22071-001-V8

ISSUE DATE 24 October 2023

SHEET NO. 04 OF 07

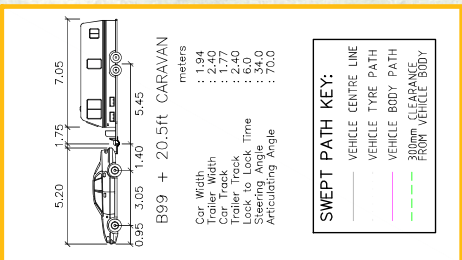
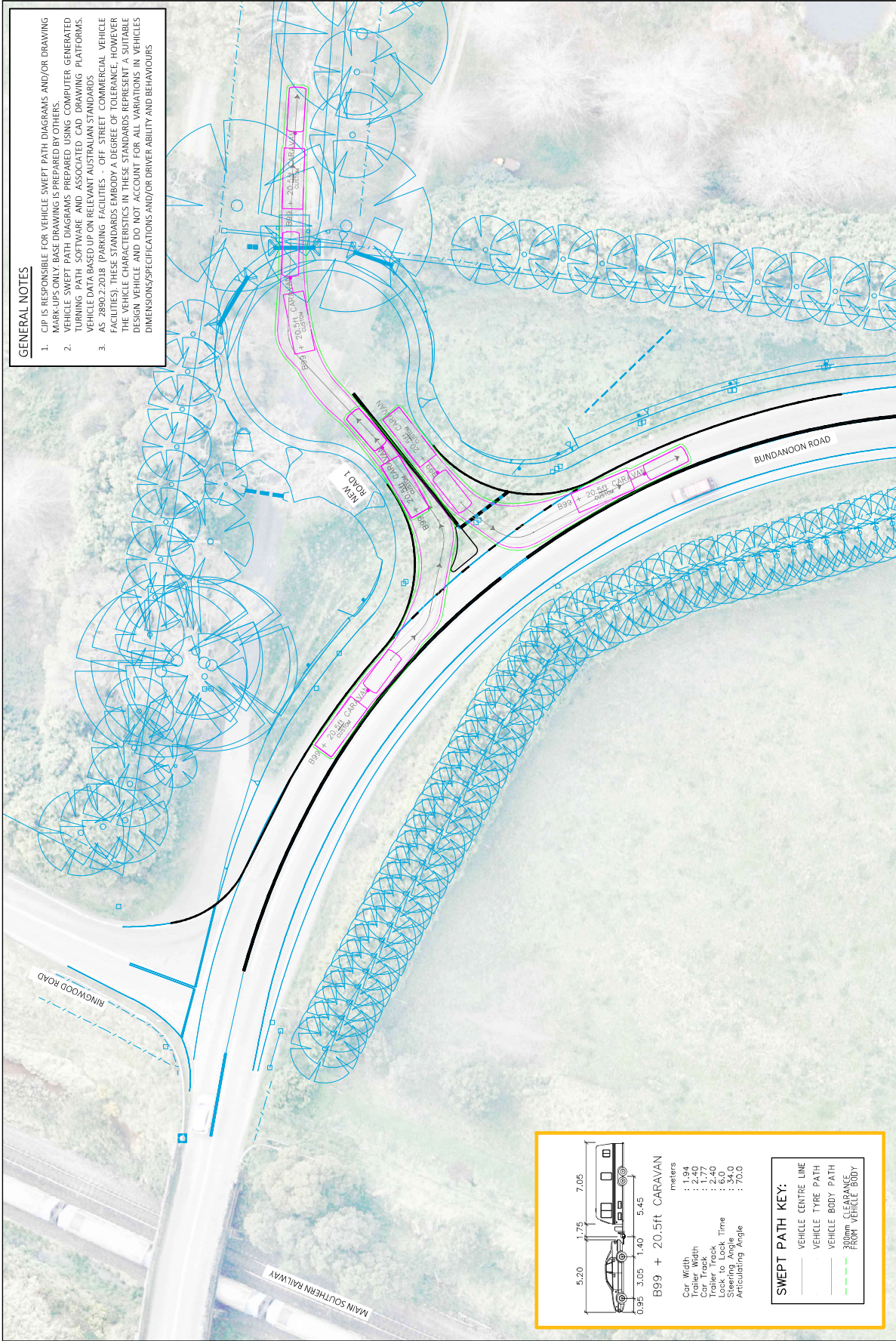
DRAWN BY CAD

REVIEWED BY C.PALMER



# AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING THURSDAY 16 MAY 2024

- GENERAL NOTES**
1. CIP IS RESPONSIBLE FOR VEHICLE SWEEP PATH DIAGRAMS AND/OR DRAWING MARK-UPS ONLY. BASE DRAWING IS PREPARED BY OTHERS.
  2. VEHICLE SWEEP PATH DIAGRAMS PREPARED USING COMPUTER GENERATED TURNING PATH SOFTWARE AND ASSOCIATED CAD DRAWING PLATFORMS, VEHICLE DATA BASED UP ON RELEVANT AUSTRALIAN STANDARDS.
  3. AS 2890.2:2018 (PARKING FACILITIES - OFF STREET COMMERCIAL VEHICLE FACILITIES). THESE STANDARDS EMBODY A DEGREE OF TOLERANCE, HOWEVER THE VEHICLE CHARACTERISTICS IN THESE STANDARDS REPRESENT A SUITABLE DESIGN VEHICLE AND DO NOT ACCOUNT FOR ALL VARIATIONS IN VEHICLES DIMENSIONS/SPECIFICATIONS AND/OR DRIVER ABILITY AND BEHAVIOURS.



SCALE 1:400 @ A3  
DRAWING NO. 22071-001-V8  
ISSUE DATE 24 October 2023  
SHEET NO. 05 OF 07  
DRAWN BY CAD  
REVIEWED BY C.FALMER



LOT 2 BUNDANOON RD, EXETER  
PROPOSED NEW ROAD 1  
SWEEP PATH ASSESSMENT

**WARNING**  
THIS IS A PRELIMINARY PLAN  
FOR INFORMATION PURPOSES  
ONLY. IT IS NOT TO BE USED  
FOR CONSTRUCTION.

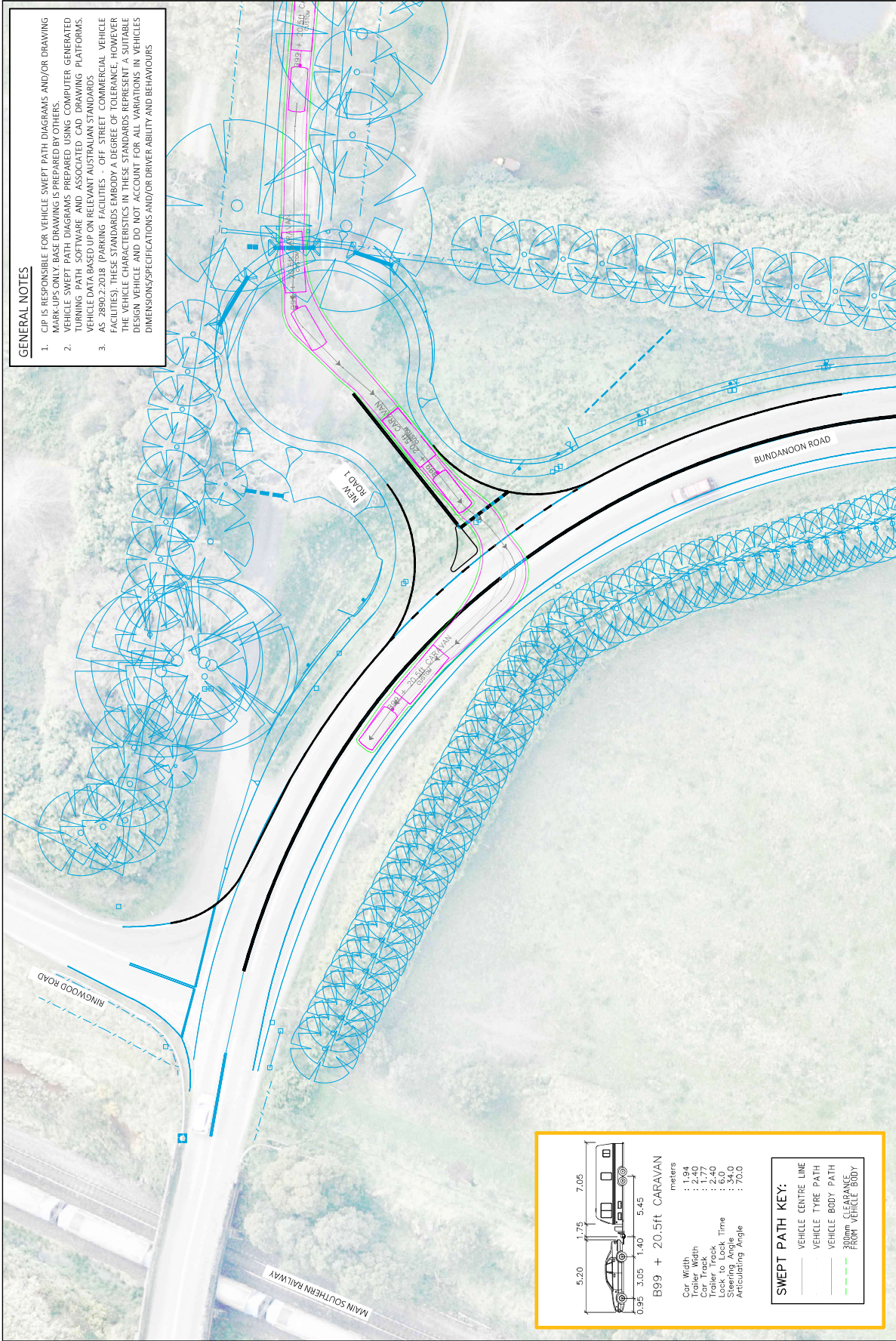
**PRELIMINARY PLAN**  
FOR INFORMATION PURPOSES  
ONLY. IT IS NOT TO BE USED  
FOR CONSTRUCTION.

CJP Consulting Engineers  
PO Box 1184  
Hunters Hill NSW 2110  
M: 0415 256 233  
E: info@cjpconsultingengineers.com.au





# AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING THURSDAY 16 MAY 2024



**GENERAL NOTES**

1. CIP IS RESPONSIBLE FOR VEHICLE SWEEP PATH DIAGRAMS AND/OR DRAWING MARK-UPS ONLY. BASE DRAWING IS PREPARED BY OTHERS.
2. VEHICLE SWEEP PATH DIAGRAMS PREPARED USING COMPUTER GENERATED TURNING PATH SOFTWARE AND ASSOCIATED CAD DRAWING PLATFORMS. VEHICLE DATA BASED UP ON RELEVANT AUSTRALIAN STANDARDS.
3. AS 2890.2:2018 (PARKING FACILITIES - OFF STREET COMMERCIAL VEHICLE FACILITIES). THESE STANDARDS EMBODY A DEGREE OF TOLERANCE, HOWEVER THE VEHICLE CHARACTERISTICS IN THESE STANDARDS REPRESENT A SUITABLE DESIGN VEHICLE AND DO NOT ACCOUNT FOR ALL VARIATIONS IN VEHICLES DIMENSIONS/SPECIFICATIONS AND/OR DRIVER ABILITY AND BEHAVIOURS.

	<p><b>B99 + 20.5ft CARAVAN</b> meters</p> <ul style="list-style-type: none"> <li>Car Width : 1.94</li> <li>Trailer Width : 2.40</li> <li>Car Track : 1.77</li> <li>Trailer Track : 2.40</li> <li>Steering Angle : 34.0</li> <li>Articulating Angle : 70.0</li> </ul>
<p><b>SWEEP PATH KEY:</b></p> <ul style="list-style-type: none"> <li>VEHICLE CENTRE LINE</li> <li>VEHICLE TYRE PATH</li> <li>VEHICLE BODY PATH</li> <li>300mm CLEARANCE FROM VEHICLE BODY</li> </ul>	

**WARNING**  
THIS IS A PRELIMINARY PLAN FOR INFORMATION PURPOSES ONLY. THIS PLAN IS NOT TO BE USED FOR CONSTRUCTION.

**PRELIMINARY PLAN**  
FOR INFORMATION PURPOSES ONLY. THIS PLAN IS NOT TO BE USED FOR CONSTRUCTION.

**CJP Consulting Engineers**  
PO Box 1184  
Hunters Hill NSW 2110  
M: 0415 256 233  
E: info@cjpconsultingengineers.com.au

**LOT 2 BUNDANOON RD, EXETER**  
**PROPOSED NEW ROAD 1**  
**SWEEP PATH ASSESSMENT**

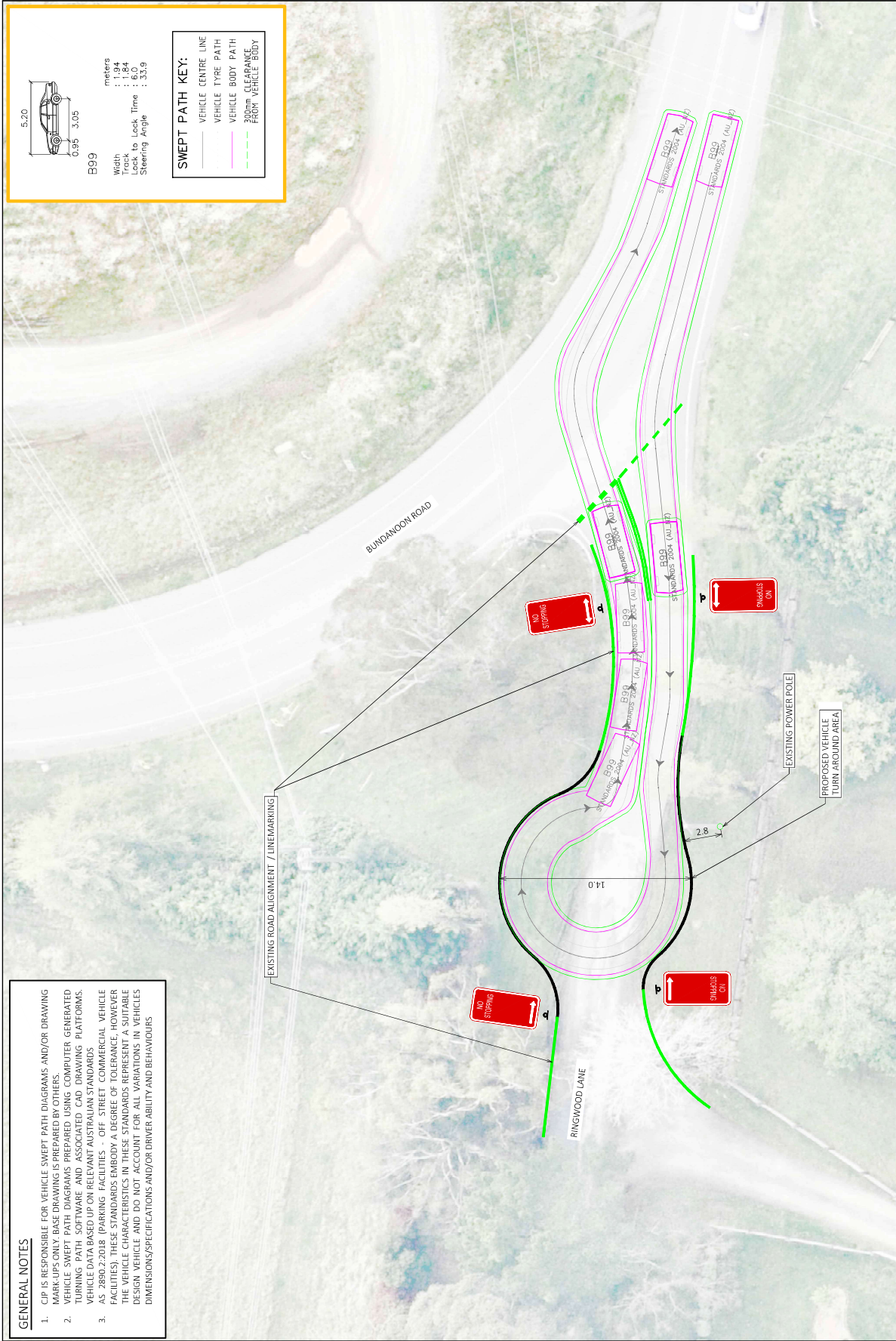
**DRAWING NO. 22071-001-V8**  
**ISSUE DATE 24 October 2023**

**SHEET NO. 06 OF 07**  
**DRAWN BY CAD**  
**REVIEWED BY C.PALMER**

**SCALE 1:400 @ A3**



# AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING THURSDAY 16 MAY 2024



### GENERAL NOTES

1. CJP IS RESPONSIBLE FOR VEHICLE SWEEP PATH DIAGRAMS AND/OR DRAWING MARK-UPS ONLY. BASE DRAWING IS PREPARED BY OTHERS.
2. VEHICLE SWEEP PATH DIAGRAMS PREPARED USING COMPUTER GENERATED TURNING PATH SOFTWARE AND ASSOCIATED CAD DRAWING PLATFORMS. VEHICLE DATA BASED UP ON RELEVANT AUSTRALIAN STANDARDS.
3. AS 2890.2:2018 (PARKING FACILITIES - OFF STREET COMMERCIAL VEHICLE FACILITIES). THESE STANDARDS EMBODY A DEGREE OF TOLERANCE, HOWEVER THE VEHICLE CHARACTERISTICS IN THESE STANDARDS REPRESENT A SUITABLE DESIGN VEHICLE AND DO NOT ACCOUNT FOR ALL VARIATIONS IN VEHICLES DIMENSIONS/SPECIFICATIONS AND/OR DRIVER ABILITY AND BEHAVIOURS.

**B99**

Width : 1.94  
Track : 1.84  
Steering Angle : 33.9

**SWEPT PATH KEY:**

- VEHICLE CENTRE LINE
- VEHICLE TYRE PATH
- VEHICLE BODY PATH
- 300mm CLEARANCE FROM VEHICLE BODY

**CJP Consulting Engineers**  
PO Box 1184  
Hunters Hill NSW 2110  
M: 0415 256 233  
E: info@cjpconsultingengineers.com.au

**WARNING**  
THIS IS A PRELIMINARY PLAN. IT IS NOT TO BE USED FOR CONSTRUCTION OR AS A BASIS FOR ANY DECISIONS.

**PRELIMINARY PLAN**  
FOR REVISIONS PURPOSES ONLY. NOT TO BE USED FOR CONSTRUCTION.

**LOT 2 BUNDANOON RD, EXETER**  
**PROPOSED TURN AROUND AREA**  
**SWEPT PATH ASSESSMENT**

SCALE 1:250 @ A3

DRAWING NO. 22071-001-V8

ISSUE DATE 24 October 2023

SHEET NO. 07 OF 07

DRAWN BY: CAD

REVIEWED BY: CALMER

## **Proposed Access Works - Lot 2, DP1275829**

### **Road Safety Audit**

Detailed Design Stage

1<sup>st</sup> November 2023

JN23040\_Report01 Rev03 - CDS 267 Bundanoon Detailed

On Behalf of

**Civil Development Solutions**



Ph: 0405 345 124  
[admin@amwc-rsa.com](mailto:admin@amwc-rsa.com)  
[www.amwc-rsa.com](http://www.amwc-rsa.com)  
ABN: 13 619 698 985



## NSW RSA Register Details

<b>Final Signoff Date</b>	05/05/2023
<b>Title of Audit</b>	Proposed Access Works - Lot 2, DP1275829
<b>Location of Audit</b>	Exeter
<b>TfNSW Project?</b>	No
<b>Project Description</b> <i>(Max 300 char)</i>	The aim of this project is to construct a new access to Lot 2, DP 1275829 from Bundanoon Road in Exeter
<b>Purpose of Audit</b> <i>(Max 300 char)</i>	The aim of this Road Safety Audit (RSA) is to assess the detailed design plans in the context of the existing conditions, and the interface between existing and proposed works
<b>State of Audit</b>	NSW
<b>Stage of Audit</b>	Detailed Design Stage
<b>Client Company</b>	Civil Development Solutions
<b>Client Contact</b>	Richard Anderson
<b>Client Phone</b>	02 4862 1277
<b>Client Email</b>	<a href="mailto:richard@cdsolutions.com.au">richard@cdsolutions.com.au</a>
<b>Audit Team Lead</b>	Aaron Walton
<b>Audit Team Member</b>	Tracey Norberg

## Table of Contents

<b>NSW RSA Register Details</b>	<b>2</b>
<b>1 Project Description</b>	<b>4</b>
<b>2 Study Area</b>	<b>4</b>
<b>3 Auditable Data</b>	<b>4</b>
<b>4 Previous Audits</b>	<b>4</b>
<b>5 Audit Stage</b>	<b>5</b>
<b>6 Exclusions</b>	<b>5</b>
<b>7 Audit Team</b>	<b>5</b>
<b>8 Audit Program</b>	<b>5</b>
<b>9 Audit Risk Assessment Technique</b>	<b>6</b>
<b>10 Audit Findings</b>	<b>7</b>
<b>11 Formal Statement</b>	<b>13</b>

## 1 Project Description

---

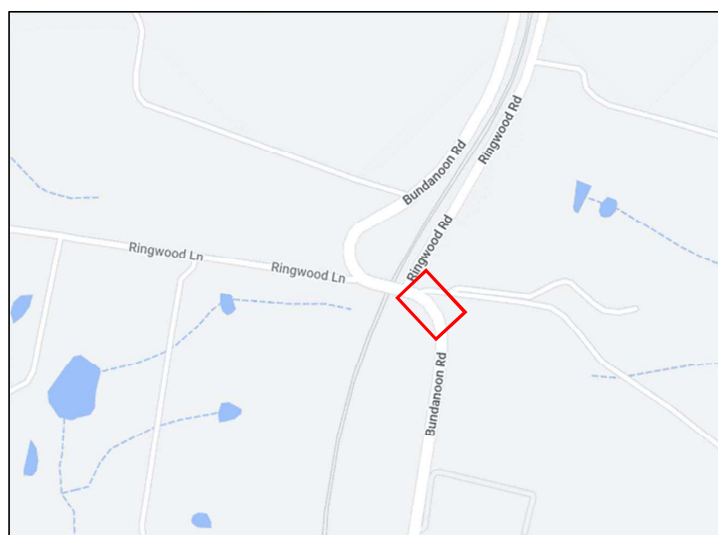
The aim of this project is to construct a new access to Lot 2, DP 1275829 from Bundanoon Road in Exeter. Proposed works include pavement shoulder widening, construction of a new access road and terminating cul-de-sac, existing access reconstruction, signage, line marking and drainage.

The aim of this Road Safety Audit (RSA) is to assess the detailed design plans in the context of the existing conditions, and the interface between existing and proposed works.

## 2 Study Area

---

The general audit location is shown below.



Source – *snazzymaps.com*

## 3 Auditable Data

---

The following data was referenced during the audit:

- > Proposed Access Works – Lot 2, DP1275829 – ‘Mittabah’ Bundanoon Rd – Exeter
  - (Rev A - 11/10/2021)
- > Lot 2 Bundanoon Road, Exeter – Existing & Proposed Signage and Linemarking – Concept Layout
  - (Rev V1 - 10/04/2023)

## 4 Previous Audits

---

The audit team has previously undertaken an Existing Stage Road Safety Audit for this project. Findings raised in previous audits have not been raised again in this audit.



## 5 Audit Stage

A Detailed Stage Audit was carried out on the 16<sup>th</sup> of April 2023 including a desktop assessment of the auditable data. A site visit of proposed works was carried out during an Existing Stage Audit on the 25<sup>th</sup> of October 2022 during day and night conditions. At the time of the site visit the weather conditions were clear and the traffic conditions were light.

The audit was generally undertaken in accordance with 'TfNSW Guidelines for Road Safety Audit Practices (2011)' and 'Austroads: Guide to Road Safety Part 6 (2022)'.

## 6 Exclusions

At the time of the audit there were no exclusions presented to the audit team.

## 7 Audit Team

The audit team and client details are shown below.

**Table 7-1 Audit Team & Client Details**

Role	Name	
<b>Client (Sponsor)</b>	Civil Development Solutions	
<b>Client Contact</b>	Richard Anderson	
<b>Client Email</b>	<a href="mailto:richard@cdsolutions.com.au">richard@cdsolutions.com.au</a>	
<b>Lead Auditor</b>	Aaron Walton	RSA-02-0501 - Level 3 Auditor
<b>Lead Auditor Email</b>	<a href="mailto:admin@amwc-rsa.com">admin@amwc-rsa.com</a>	
<b>Team member</b>	Tracey Norberg	RSA-02-0964 - Level 3 Auditor

## 8 Audit Program

The audit program details are shown below.

**Table 8-1 Audit Program**

Activity	Date	Attendees
<b>Opening Meeting</b>	28/08/2022	Aaron Walton, Richard Anderson
<b>Site Inspection</b>	25/10/2022	Aaron Walton, Tracey Norberg
<b>Detailed Design Audit</b>	16/04/2023	Aaron Walton, Tracey Norberg
<b>Draft Report Internal Review</b>	17/04/2023	RSA Report (Rev00)
<b>Draft Report External Responses</b>	18/04/2023	RSA Report (Rev01)
<b>Completion Meeting</b>	03/05/2023	Aaron Walton, Richard Anderson
<b>Final Report</b>	05/05/2023	RSA Report (Rev02)
<b>Final Report - Updated Responses</b>	01/11/2023	RSA Report (Rev03)

## 9 Audit Risk Assessment Technique

For each of the safety issues identified, the level of risk has been determined. The tables below are extracted from Austroads: Guide to Road Safety Part 6 (2022) and have been used in the assessment of risk for this audit.

**Table 9-1 Incident Likelihood**

Likelihood	Description
<b>Almost Certain</b>	Once per quarter
<b>Likely</b>	Once per quarter to once per year
<b>Possible</b>	Once per year to once every three years
<b>Unlikely</b>	Once every three years to once every seven years
<b>Rare</b>	Less than once every seven years

**Table 9-2 Incident Severity**

Severity	Description
<b>Fatal</b>	At scene or within 30 days of the crash
<b>Serious</b>	Admitted to hospital
<b>Moderate</b>	Major first aid and/or presents to hospital (not admitted)
<b>Minor</b>	Minor first aid
<b>Insignificant</b>	Property damage

**Table 9-3 Incident Priority for Mitigation**

Priority	Description
<b>Extreme</b>	Must be corrected regardless of cost.
<b>High</b>	Should be corrected or the risk significantly reduced, even if the treatment cost is high
<b>Medium</b>	Should be corrected or the risk significantly reduced, if the treatment cost is moderate but not high
<b>Low</b>	Should be corrected or the risk reduced if the treatment cost is low
<b>Negligible</b>	No action required

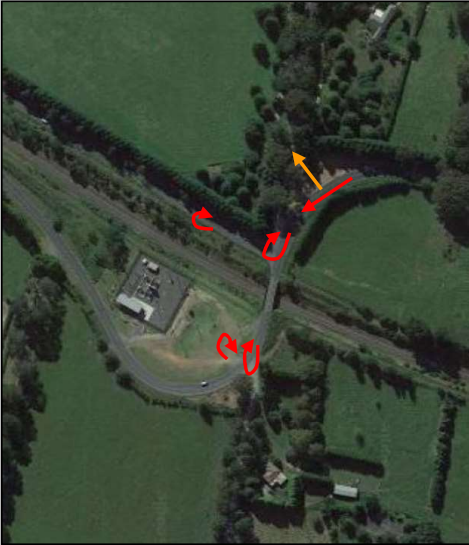
**Table 9-4 Resulting Level of Risk Matrix**

	Insignificant	Minor	Moderate	Serious	Fatal
Almost Certain	Medium	High	High	Extreme	Extreme
Likely	Medium	Medium	High	Extreme	Extreme
Possible	Low	Medium	High	High	Extreme
Unlikely	Negligible	Low	Medium	High	Extreme
Rare	Negligible	Negligible	Low	Medium	High

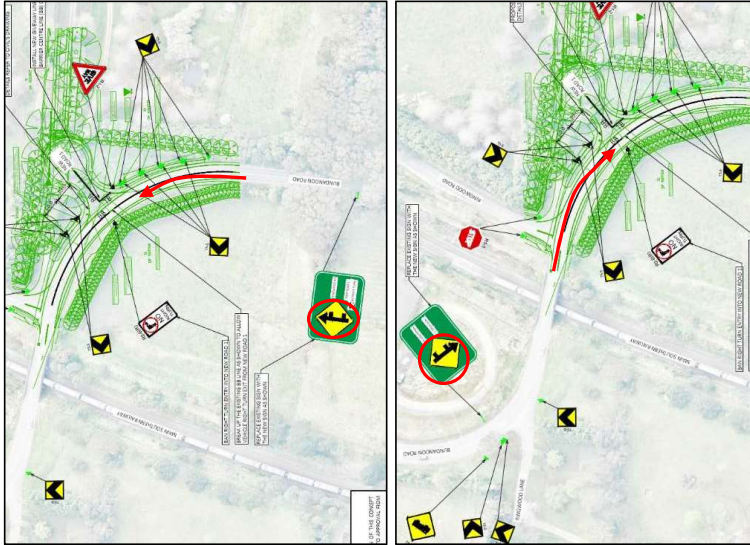
Safe System  
crash outcome  
threshold

## 10 Audit Findings

**Table 10-1 Audit Findings**

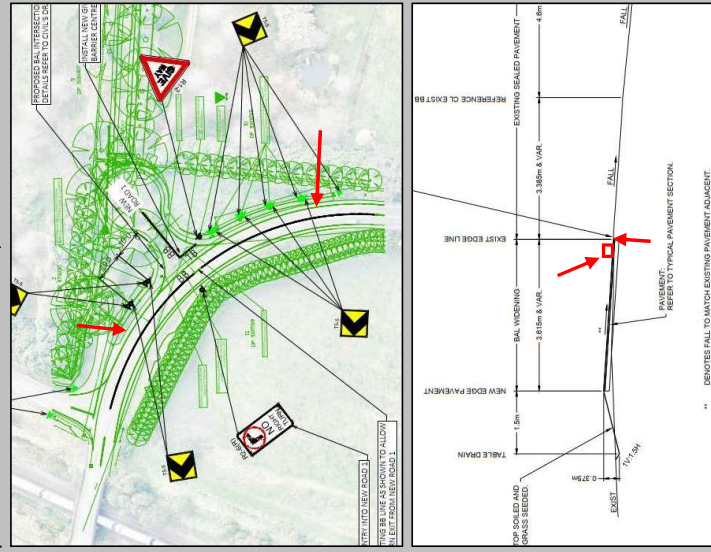
Item	Safety Hazard Finding	Risk Assessment	Project Manager Response
1. No Right Turn New Road 01	<p>There is a right turn restriction for northbound vehicles on Bundanoon Road entering New Road 01.</p> <p>On site the audit team could not locate a safe turn around point past the right turn restriction.</p> <p>There is a risk that a northbound motorist may attempt unsafe movements such as U-turns at intersection points, property accesses or in locations of narrow road shoulder or restricted sight distance to turn around and enter New Road 01 resulting in rear end or side impact collisions.</p> 	<p>Likelihood - Possible Severity - Serious Priority - High</p>	<p>The analysis and design of the proposed intersection resulted in a proposal for no right turn into the site from Bundanoon Road. This was as a result of limited SISD distance from the south limitations of providing a BAR on the existing road. Due to the very limited amount of traffic movements that is expected to occur for this movement it was considered reasonable and safer to ban the right turn movement. It is our opinion there is opportunity for the provision of a turning head arrangement a short distance along Ringwood Lane to allow for vehicles to turnaround and this risk can be significantly downgraded. This turnaround location is only 190m to the west of the proposed intersection location and has good sight distance in both directions. The proposed turnaround area on Ringwood Lane is setback approximately 30m from the Bundanoon Road intersection, thereby achieving a storage capacity of 4 cars. This is unlikely to ever reach full storage capacity, such that the turnaround area should be clear at all times.</p>



Item	Safety Hazard Finding	Risk Assessment	Project Manager Response
<p><b>2.</b> Intersection Signs On Approaches</p>	<p>There are advanced intersection signs proposed on the northern and southern approaches of Bundanoon Road. The signs indicate intersection points on a straight alignment however the alignment is curved. There is a risk that a motorist may not anticipate the intersection points to be on the curve resulting in rear end collisions with a turning vehicle.</p> 	<p>Likelihood - Unlikely Severity - Moderate Priority - Medium</p>	<p>The proposed signage plan has been modified to indicate curved roads rather than straight roads.</p>

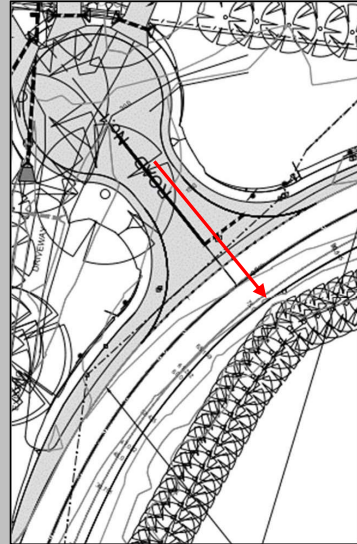
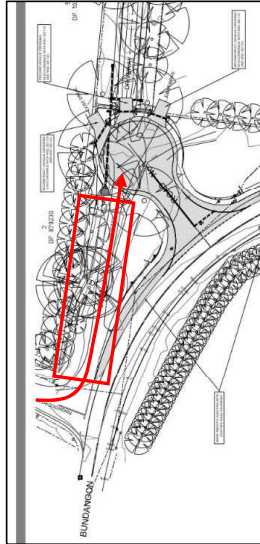
**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**

Road Safety Audit  
Proposed Access Works - Lot 2, DP1275829

Item	Safety Hazard Finding	Risk Assessment	Project Manager Response
<p><b>3.</b> Line Marking Back of Curve</p>	<p>There is a pavement joint located at the edge line on the outside of the curve. The edge line at the pavement joint is not proposed to be reinstated. There is a risk that the edge line at the pavement joint may be damaged or removed during pavement works decreasing a motorist's awareness to the curved road alignment resulting in run-off-road incidents. Additionally, it is unclear to the audit team of any raised pavement markers to be installed/reinstated.</p> 	<p>Likelihood - Rare Severity - Moderate Priority - Medium</p>	<p>Drafting issue – Edge line is to be retained – plans to be updated to state edge line and raised pavement markers to be reinstated. Intersection to have C1 continuity line.</p>

**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**


Item	Safety Hazard Finding	Risk Assessment	Project Manager Response
<p><b>4.</b> Existing Access Existing Access</p>	<p>There is no data provided for retention or removal of the existing access. There is a risk that motorists may attempt to use the existing access point, particularly with the right turn restriction for northbound motorists. Risks associated with the existing access point have been raised in the existing stage audit report including angle of intersection, conflict points, sight obstructions and overhanging branches.</p>	<p>Likelihood - Likely Severity - Moderate Priority - High</p>	<p>Intention is for the existing access to be removed. Plans to be updated to reflect this.</p>
<p><b>5.</b> Contours New Road 01</p>	<p>There is no proposed contour data provided to the audit team. It is unclear to the audit team of proposed pavement flow paths. There is a risk that the length of a flow path from the crest of New Road 01 down to and across Bundanoon Road may create a deep flow of water resulting in aquaplaning incidents.</p>	<p>Likelihood - Unlikely Severity - Minor Priority - Low</p>	<p>This is unlikely (as noted) – the high point is only 19m from the cl of the road and the road is crowned. Limited flow path over existing Bundanoon Road however analysis can be undertaken to confirm.</p>





Item	Safety Hazard Finding	Risk Assessment	Project Manager Response
6. Drainage Extent of Works	<p>There is insufficient contour, flow direction or drainage connectivity provided between the existing and the design. It is unclear to the audit team where table drains discharge. There is a risk that water flows may pond in the table drain or against the pavement layers resulting in pavement failure and destabilisation incidents for vehicles and motorcycles.</p> <p>There is a risk that water flows may overtop the formation and be directed across the travel lane resulting in aquaplaning incidents.</p>	<p>Likelihood - Unlikely Severity - Minor Priority - Low</p>	<p>Intent is for flows to be directed around the back of the cul-de-sac utilising driveway crossing culverts, further investigation can be undertaken to confirm drainage flow paths. Existing culvert under driveway to the west of the proposed cul-de-sac can be retained to drain low area. Cul-de-sac area is all in fill. If necessary a box culvert can be placed to align with the low point in the kerb return where there is an existing drainage depression shown on the eastern side of the cul-de-sac (approx. Ch 30).</p>
7. Batter Slope Extent of Works	<p>There is no data provided to the audit team to indicate the batter slopes throughout the extent of works. There is a risk that a steep batter at the back of a curve may result in vehicle roll over incidents for errant vehicles.</p>	<p>Likelihood - Possible Severity - Serious Priority - High</p>	<p>Disagree – the plan shows a typical section with 1.5m batter to 0.375 deep table drain – indicating 1:4 batters. We note that all batters have been designed at 1:4 or flatter which is compliant batter for run off areas. Plans to be updated to show this batter on typical section however it is clear from the cross sections of Bundanoon Rd that this is the case.</p>

**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**

Item	Safety Hazard Finding	Risk Assessment	Project Manager Response
<b>8.</b> Pavement Joint Extent of Works	The proposed pavement widening creates a vertical face with the existing pavement. There is no proposed treatment for the pavement joint. There is a risk overtime that a vertical pavement joint may open to water creating pavement failure resulting in destabilisation incidents for vehicles and motorcycles.	Likelihood - Unlikely Severity - Minor Priority - Low	It would be the intention to provide a staggered joint between the layers. Drawings to be updated accordingly.
<b>9.</b> Parking Cul-de-sac	 <p>There is parking permitted within the manoeuvring area of the cul-de-sac.                      There is a risk that a parked vehicle may restrict the movement of through vehicles or turning vehicles resulting in sideswipe collisions.</p>	Likelihood - Unlikely Severity - Insignificant Priority - Negligible	The proposed signage plan has been modified to indicate No Stopping restrictions within the cul-de-sac turning head.

## 11 Formal Statement

---

We, the undersigned, declare that we have reviewed the site and data listed in this report and identified the safety and operational deficiencies above.

It should be noted that while every effort has been made to identify potential safety hazards, no guarantee could be made that every deficiency has been identified.

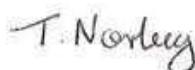
A project sponsor is under no obligation to accept the findings outlined in this audit report. This report simply provides the opportunity to review potential safety issues highlighted by the auditors.

This audit will be recorded on the NSW Register of Road Safety Auditors and the project sponsor should expect email notification from the register to confirm the audit has been carried out.

We recommend that points of concern be investigated, and necessary corrective actions undertaken.



**Aaron Walton**  
*Level 3 Road Safety Auditor*  
*Team Leader*



**Tracey Norberg**  
*Level 3 Road Safety Auditor*  
*Team Member*



---

## **6.10 Traffic Arrangements for Proposed Development on Sally Corner Road, Sutton Forest**

---

**Report Author: Traffic Engineer**

**Authoriser: Karin Targa**

### **PURPOSE**

---

To assess the traffic arrangements for a proposed food outlet development on Sally's Corner Road, Sutton Forest.

### **RECOMMENDATION**

---

***THAT the civil design plans and associated traffic arrangements designed by Richmond and Ross Consulting Engineers (Plan No. 230139) and SLR Consulting (Plan No. 610.31088) be recommended for approval on the condition that the speed limit is reduced from 90km/h to 60km/h.***

### **REPORT**

---

There is a proposed development to build two food outlets on Sally Corner Road, opposite the current Heatherbrae Pies business.

There are two driveways proposed. The eastern most driveway is currently in the 90km/h zone, and the western most driveway is in a 60km/h zone.

The developer has applied to TfNSW to extend the 60km/h zone approximately 250m to the east. This request is currently being reviewed.

The eastern driveway proposes to restrict the right turn into the car park and allow the right turn out of the car park.

The sight distance for vehicles exiting the eastern driveway, looking east is 130m. The minimum sight distance stated in Australian Standards for an access driveway at 90km/h is 130m.

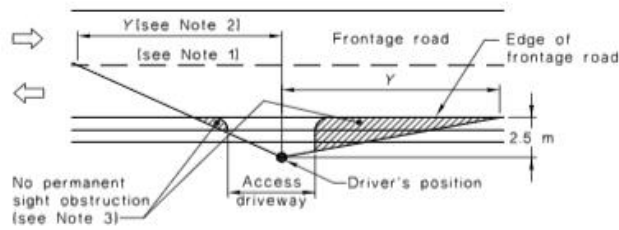


**Image 1 - Aerial image showing traffic facility arrangement and turning paths**

**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**

AS/NZS 2890.1:2004

32



Frontage road speed (Note 4) km/h	Distance (Y) along frontage road m		
	Access driveways other than domestic (Note 5)		Domestic property access (Note 6)
	Desirable 5 s gap	Minimum SSD	
40	55	35	30
50	69	45	40
60	83	65	55
70	97	85	70
80	111	105	95
90	125	130	Use values from 2 <sup>nd</sup> and 3 <sup>rd</sup> columns
100	139	160	
110	153	190	

**NOTES:**

- 1 Centre-line or centre of road (undivided road), or right hand edge of right hand through lane (divided road).
- 2 A check to the left is not required at a divided road where the median is wide enough to shelter a vehicle leaving the driveway.
- 3 Parking on this side of the frontage road may need to be restricted on either side of the driveway so that the sight distance required by the above table to an approaching vehicle is not obstructed.
- 4 This is the posted or general speed limit unless the 85th percentile speed is more than 5 km/h above the limit in which case the tabulated speed nearest the 85<sup>th</sup> percentile shall be adopted.
- 5 The values in the table apply only to left turn and right turn manoeuvres into two-way roads up to four lanes wide and one-way streets regardless of width, either for a 5 s gap, desirable at lower frontage road speeds, or minimum stopping sight distance based on 2 s reaction time.  
Crossing manoeuvres (e.g. from an access opposite the stem of a T-junction) over four lanes or more, and turning manoeuvres into a six lane two-way road would require longer gaps unless there was a median wide enough to store a vehicle and allow a two stage manoeuvre.
- 6 These distances are based on stopping sight distances with reaction time of 1.5 s for traffic approaching along the frontage road and are applicable to a frontage road speed of up to 80 km/h only. Wherever practicable sight distance provided at domestic property accesses should meet the values given in the second or third columns of the Table.
- 7 When checking sight distance the driver's eye height and the height of the object (approaching vehicle) are to be taken as 1.15 m above the road surface.

**FIGURE 3.2 SIGHT DISTANCE REQUIREMENTS AT ACCESS DRIVEWAYS**

The safe intersection sight distance of a car stopping to turn right into the eastern driveway is 214m allowing for 2 seconds of reaction time. The site does not allow for the required site distance therefore the right turn in proposed to be restricted. A median island and No Right Turn are proposed to restrict the right turn. Vehicle volumes accessing the site from the east are expected to be low. Vehicles assessing from the east will use the western driveway to turn right into the development.



**AGENDA OF THE LOCAL TRAFFIC COMMITTEE MEETING  
THURSDAY 16 MAY 2024**

**Table 3.2: Safe intersection sight distance (SISD) and corresponding minimum crest vertical curve size for sealed roads ( $S < L$ )**

Design speed (km/h)	Based on safe intersection sight distance for cars <sup>(1)</sup> $h_1 = 1.1$ ; $h_2 = 1.25$ , $d = 0.36$ <sup>(2)</sup> ; Observation time = 3 sec					
	$R_T = 1.5$ sec <sup>(3)</sup>		$R_T = 2.0$ sec		$R_T = 2.5$ sec	
	SISD (m)	$K$	SISD (m)	$K$	SISD (m)	$K$
40	67	4.9	73	6	–	–
50	90	8.6	97	10	–	–
60	114	14	123	16	–	–
70	141	22	151	25	–	–
80	170	31	181	35	–	–
90	201	43	214	49	226	55
100	234	59	248	66	262	74
110	–	–	285	87	300	97
120	–	–	324	112	341	124
130	–	–	365	143	383	157

- 1 If the average grade over the braking length is not zero, calculate the safe intersection sight distance (SISD) values using the correction factors in Table 3.4 (or use Equation 2) by applying the average grade over the braking length.
- 2 A coefficient of deceleration of greater than 0.36 is not provided in this table. The provision of SISD requires more conservative values than for other sight distance models (e.g. the stopping sight distance model allows values up to 0.46 in constrained situations). This is because there is a much higher likelihood of colliding with hazards at intersections (that is, other vehicles). Comparatively, there is a relatively low risk of hitting a small object on the road (the stopping sight distance model).
- 3 A 1.5 sec reaction time is only to be used in constrained situations where drivers will be alert. Typical situations are given in Table 4.2 of AGRD Part 3. The general minimum reaction time is 2 sec.

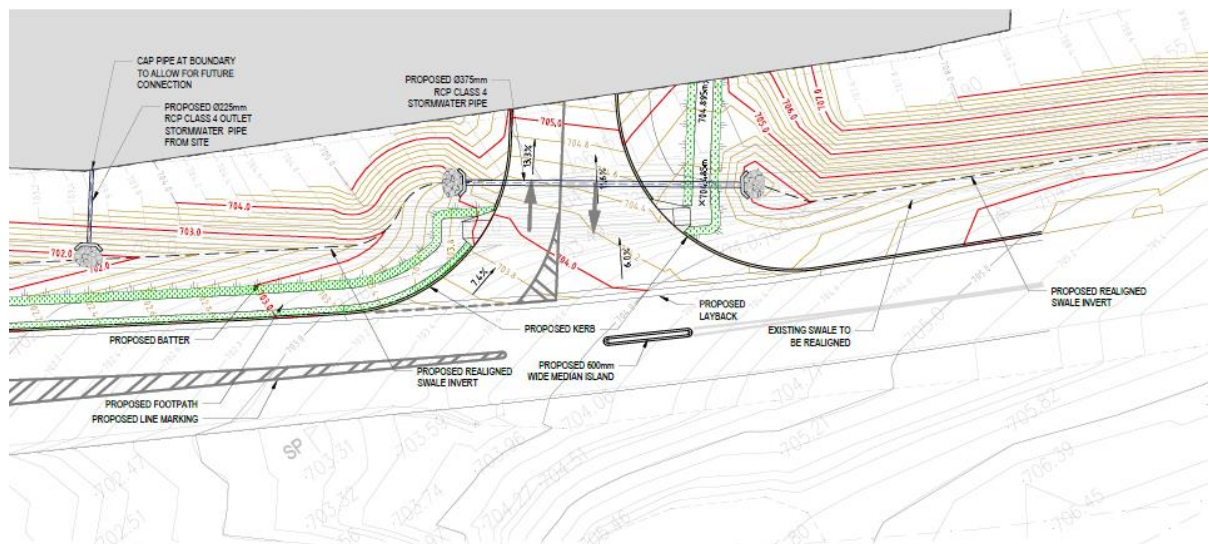
**Notes:**

$K$  is the length of vertical curve for a 1% change in grade.

To determine SISD for trucks around horizontal curves, use Equation 2 with an observation time of 2.5 sec.

Main Roads Western Australia have adopted a desirable minimum reaction time of 2.5 sec and an absolute minimum reaction time of 2.0 sec. A reaction time of 1.5 sec is not to be used in Western Australia.

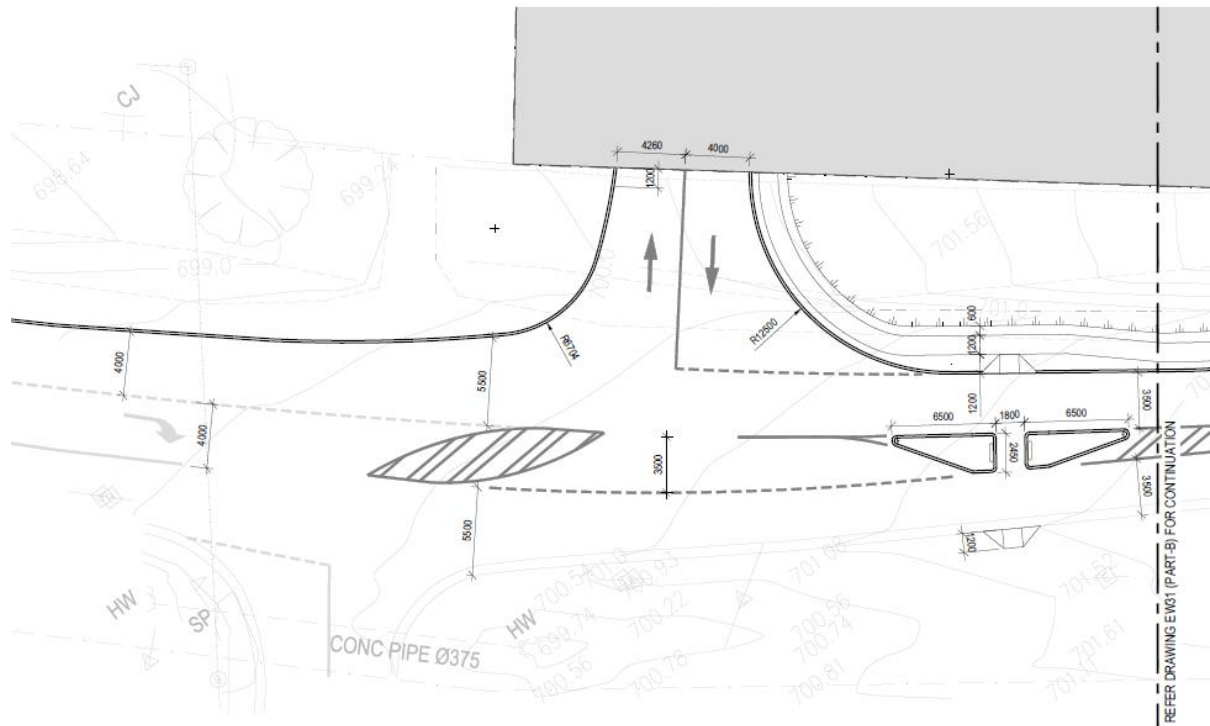
Combinations of design speed and reaction times not shown in this table are generally not used.



**Image 2 – Traffic facility arrangement for eastern driveway**

The western driveway proposes a short right turn lane with associated line marking and a pedestrian refuge. The right turn allows right turning cars to move off the through travel lane which

allows for better traffic flow and removes the possibility of a rear end accident. The turning arrangement into the development is like the right turn arrangement for the adjacent McDonalds and Heatherbrae Pies.



**Image 3 – Right turn arrangements for eastern driveway and pedestrian refuge**

The signage and line marking arrangements are attached.

## **CONCLUSION**

---

The proposed traffic arrangement for the development on Sally's Corner Road meets Australian Standards for driveway sight distances. Appropriate measures have been put in place to address the safe intersection sight distance for right turning vehicles into the eastern driveway.

Appropriate lane widths are provided. The right turn arrangement into the western driveway allows for vehicles to move from the travel lane and is similar to the turning arrangement for the adjacent food outlets.

A pedestrian footpath is provided and pedestrian refuge.

The proposed development on Sally's corner road is recommended for approval subject to the speed limit being reduced from 90km/h to 60km/h

## **ATTACHMENTS**

---

1. Sally's Corner Road Proposed Development Civil Plans [6.10.1 - 16 pages]

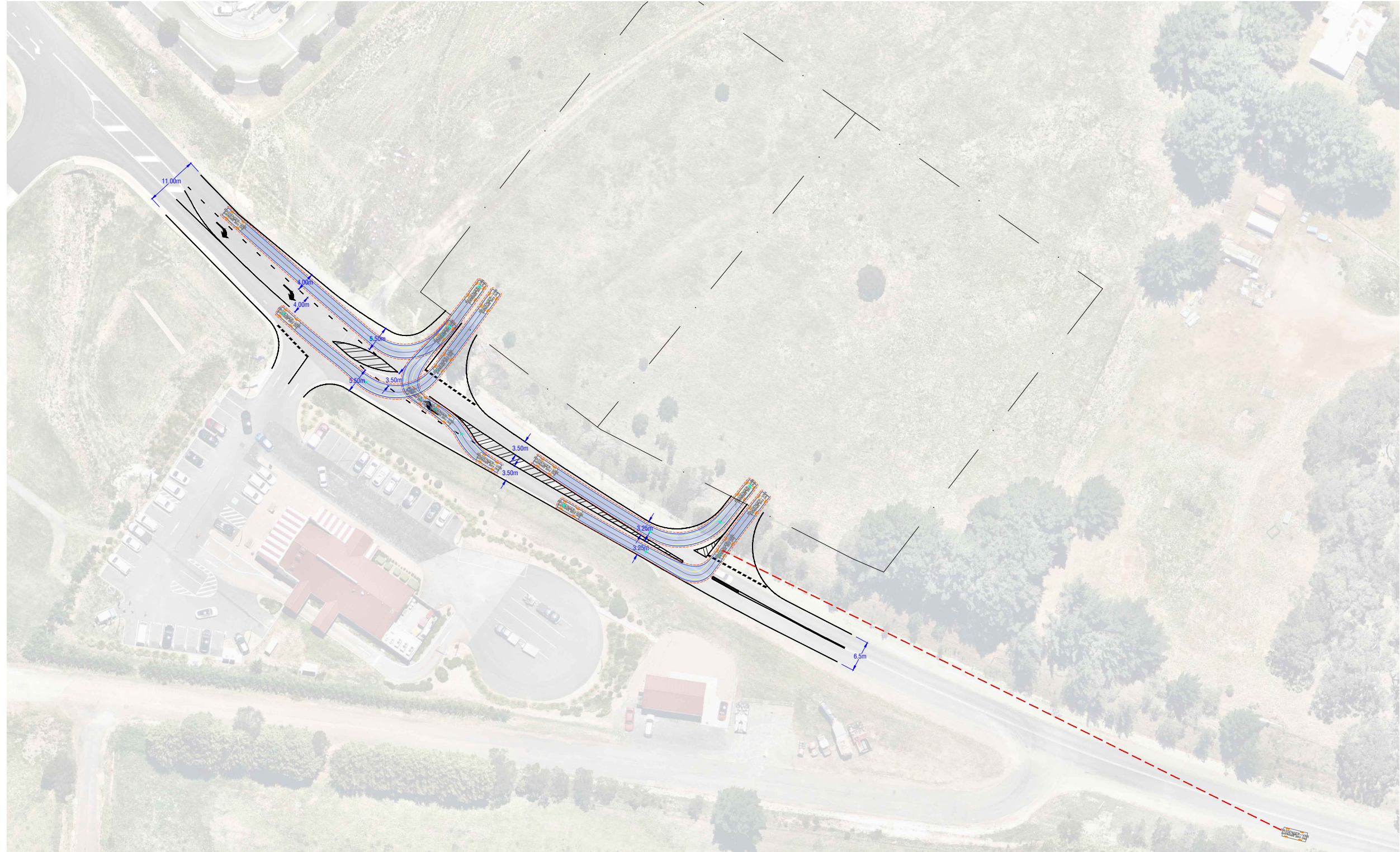
# SALLYS CORNER ROAD

SUTTON FOREST, NSW 2579

EXTERNAL CIVIL WORKS DRAWINGS		
DWG. NO.	AMDT.	DRAWING TITLE
EW00	H	COVER PAGE & DRAWING LIST
SK-01		PROPOSED RIGHT AND LEFT TURN ARRANGEMENTS (CARS) & SIGHT DISTANCE
SK-02A		PROPOSED RIGHT AND LEFT TURN ARRANGEMENTS (TRUCKS)
EW01	F	OVERALL SITE PLAN
EW10	G	EXISTING & DEMOLITION SITE PLAN PART-A
EW11	E	EXISTING & DEMOLITION SITE PLAN PART-B
EW20	G	PROPOSED SITE PLAN PART-A
EW21	F	PROPOSED SITE PLAN PART-B
EW30	E	DIMENSION & SETOUT PLAN PART-A
EW31	E	DIMENSION & SETOUT PLAN PART-B
EW40	G	LINE MARK AND SIGNAGE PLAN PART-A
EW41	G	LINE MARKING AND SIGNAGE PLAN PART-B
EW50	F	PAVEMENT PLAN PART-A
EW51	E	PAVEMENT PLAN PART-B
EW52	C	PAVEMENT DETAILS (SHEET 1 OF 2)
EW53	D	PAVEMENT DETAILS (SHEET 2 OF 2)

REV No.	COMMENTS	DATE	INIT.	PROJECT:	<b>Richmond+Ross</b> PTY LIMITED CONSULTING ENGINEERS AND PROJECT LEADERS ABN 34 001 485 436	COVER PAGE & DRAWING LIST		
B	TENDER ISSUE	13.02.24	DJ	SALLYS CORNER ROAD SUTTON FOREST NSW 2579		DATE:	MAY 2023	DRG.No.
C	FOR APPROVAL	12.04.24	DJ	<div style="border: 1px solid black; padding: 5px; display: inline-block;">S138 APPROVAL</div>	SCALE:	N.T.S.	EW00	
D	FOR APPROVAL	16.04.24	DJ		CLIENT:	JOB No.		230139
E	FOR APPROVAL	17.04.24	DJ		APS	38 WILLOUGHBY ROAD, CROWS NEST, NSW 2065 TEL : (02) 9490 9600 FAX : (02) 9438 1224		
F	FOR APPROVAL	17.04.24	DJ					
G	FOR APPROVAL	24.04.24	DJ					
H	FOR S138 APPROVAL	30.04.24	DJ					
J	FOR S138 APPROVAL	01.05.24	DJ					



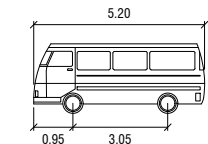


*The content contained within this document may be based on third party data. SLR Consulting Australia Pty Ltd does not guarantee the accuracy of any such information.*

Project No:	610.31088
Date:	02/05/2024
Drawn by:	Charlie Seventekin
Certified by:	Charlie Seventekin
Sheet Size:	A3
Projection:	GDA2020

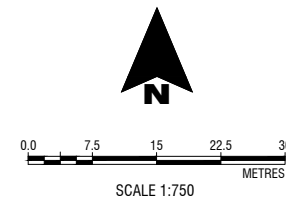
**SWEPT PATH LEGEND**

	Vehicle Path
	Vehicle Body
	Body Clearance
	Front Wheels



**PASSENGER-VAN**

Width	: 1.94
Track	: 1.84
Lock to Lock Time	: 6.0
Steering Angle	: 33.6



Australian Property Syndicates Pty Ltd

S138 Application

**Proposed Right and Left Turn Arrangements (Cars) & Sight Distance**

FIGURE SK-01

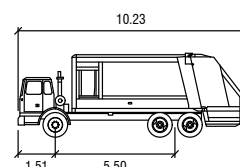




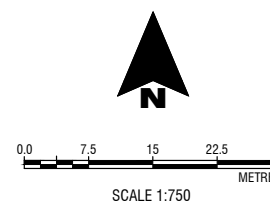
*The content contained within this document may be based on third party data. SLR Consulting Australia Pty Ltd does not guarantee the accuracy of any such information.*

Project No:	610.31088
Date:	02/05/2024
Drawn by:	Charlie Seventekin
Certified by:	Charlie Seventekin
Sheet Size:	A3
Projection:	GDA2020

**SWEPT PATH LEGEND**  
 - Vehicle Path  
 - Vehicle Body  
 - Body Clearance  
 - Front Wheels

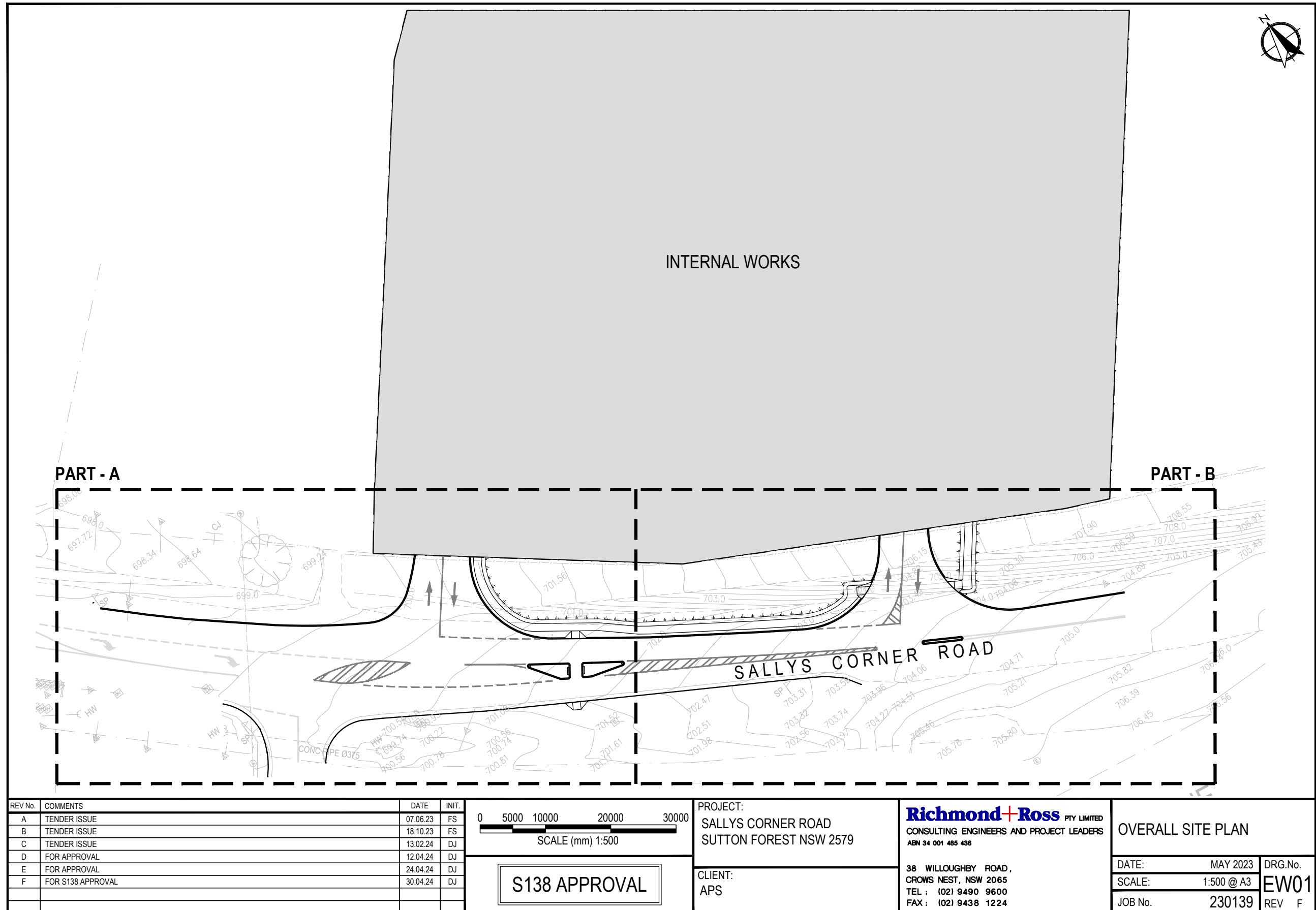


<b>BCC Rear Load RCV (ACCO 2350)</b>	
	metres
Width	: 2.50
Track	: 2.50
Lock to Lock Time	: 6.0
Steering Angle	: 40.3



Australian Property Syndicates Pty Ltd  
 S138 Application  
**Proposed Right and Left Turn Arrangements (Trucks)**  
 FIGURE SK-02A





REV No.	COMMENTS	DATE	INIT.
A	TENDER ISSUE	07.06.23	FS
B	TENDER ISSUE	18.10.23	FS
C	TENDER ISSUE	13.02.24	DJ
D	FOR APPROVAL	12.04.24	DJ
E	FOR APPROVAL	24.04.24	DJ
F	FOR S138 APPROVAL	30.04.24	DJ

0 5000 10000 20000 30000  
SCALE (mm) 1:500

**S138 APPROVAL**

PROJECT:  
SALLY'S CORNER ROAD  
SUTTON FOREST NSW 2579

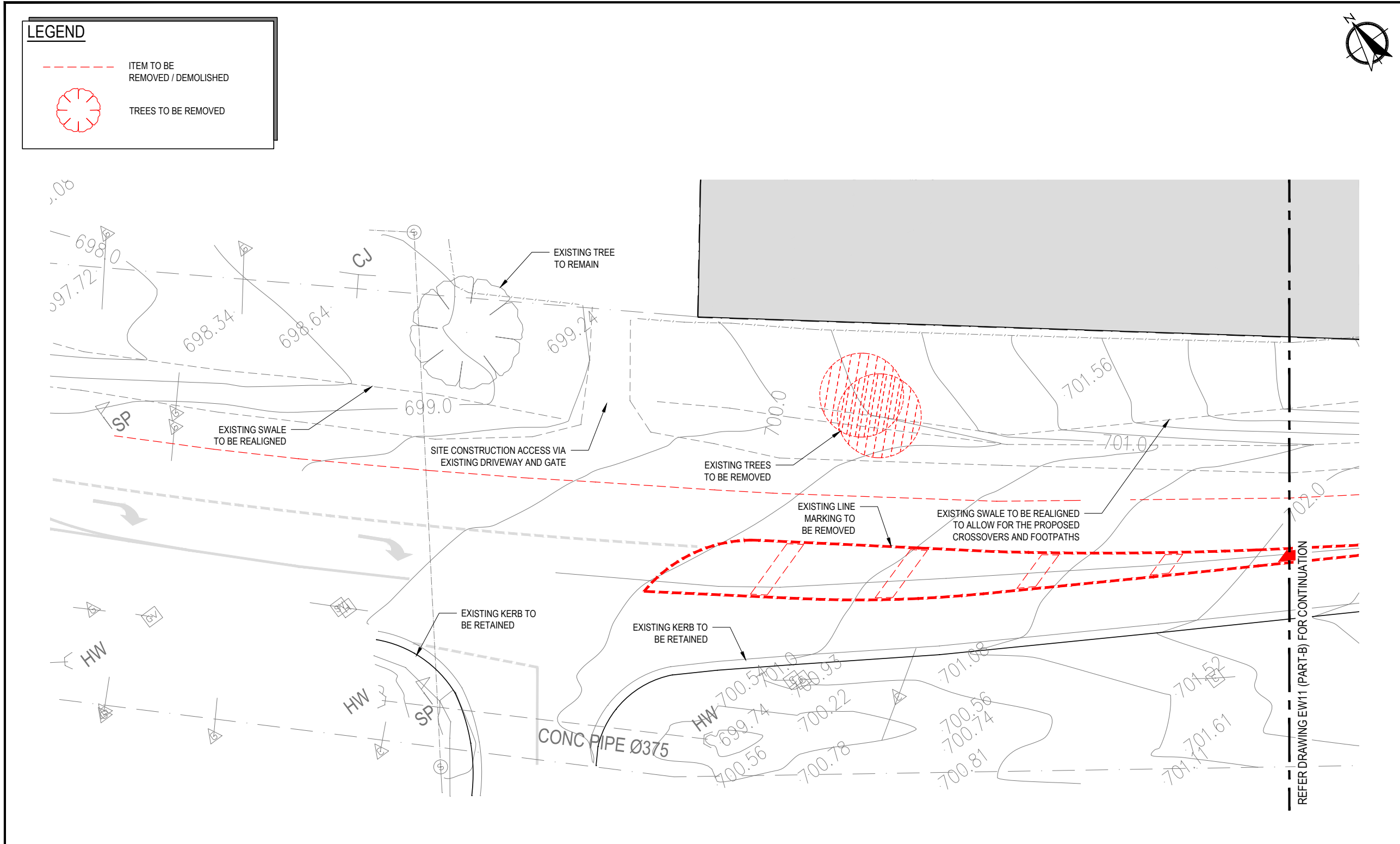
CLIENT:  
APS

**Richmond+Ross** PTY LIMITED  
CONSULTING ENGINEERS AND PROJECT LEADERS  
ABN 34 001 485 436

38 WILLOUGHBY ROAD,  
CROWS NEST, NSW 2065  
TEL : (02) 9490 9600  
FAX : (02) 9438 1224

<b>OVERALL SITE PLAN</b>	
DATE:	MAY 2023
SCALE:	1:500 @ A3
JOB No.	230139
DRG.No.	<b>EW01</b>
REV	F





REV No.	COMMENTS	DATE	INIT.
A	TENDER ISSUE	07.06.23	FS
B	TENDER ISSUE	18.10.23	FS
C	TENDER ISSUE	13.02.24	DJ
D	FOR APPROVAL	12.04.24	DJ
E	FOR APPROVAL	17.04.24	DJ
F	FOR APPROVAL	24.04.24	DJ
G	FOR S138 APPROVAL	30.04.24	DJ

0 2500 5000 10000 15000  
SCALE (mm) 1:250

**S138 APPROVAL**

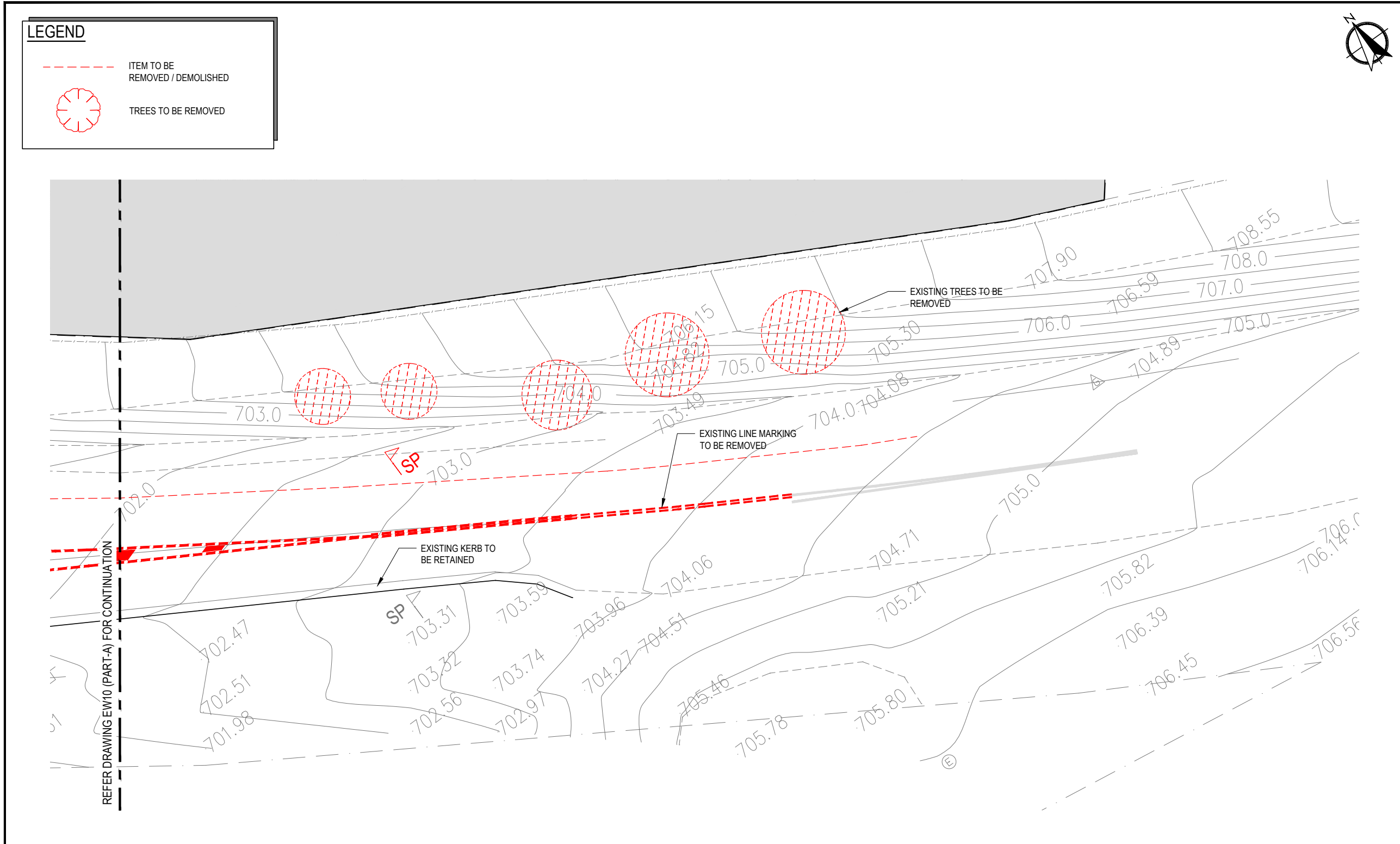
PROJECT:  
SALLYS CORNER ROAD  
SUTTON FOREST NSW 2579

CLIENT:  
APS

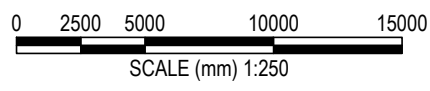
**Richmond+Ross** PTY LIMITED  
CONSULTING ENGINEERS AND PROJECT LEADERS  
ABN 34 001 485 436

38 WILLOUGHBY ROAD,  
CROWS NEST, NSW 2065  
TEL : (02) 9490 9600  
FAX : (02) 9438 1224

EXISTING & DEMOLITON SITE PLAN PART - A	
DATE:	MAY 2023
SCALE:	1:250 @ A3
JOB No.	230139
DRG.No.	EW10
REV	G



REV No.	COMMENTS	DATE	INIT.
A	TENDER ISSUE	18.10.23	FS
B	TENDER ISSUE	13.02.24	DJ
C	FOR APPROVAL	12.04.24	DJ
D	FOR APPROVAL	24.04.24	DJ
E	FOR S138 APPROVAL	30.04.24	DJ



**S138 APPROVAL**

PROJECT:  
SALLYS CORNER ROAD  
SUTTON FOREST NSW 2579

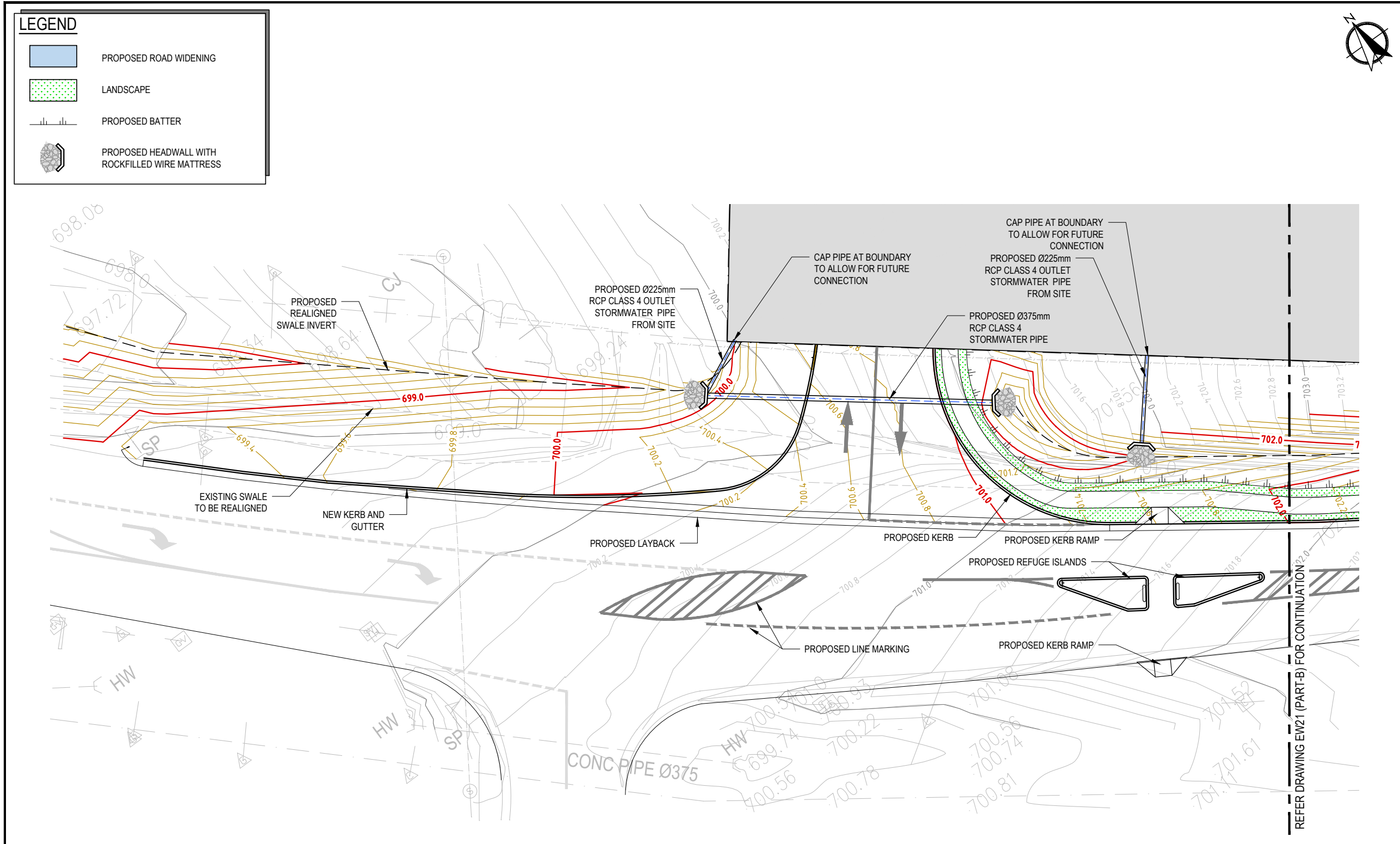
CLIENT:  
APS

**Richmond+Ross** PTY LIMITED  
CONSULTING ENGINEERS AND PROJECT LEADERS  
ABN 34 001 485 436

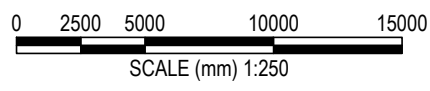
38 WILLOUGHBY ROAD,  
CROWS NEST, NSW 2065  
TEL : (02) 9490 9600  
FAX : (02) 9438 1224

EXISTING & DEMOLITON  
SITE PLAN PART - B

DATE:	MAY 2023	DRG.No.	
SCALE:	1:250 @ A3	<b>EW11</b>	
JOB No.	230139	REV	E



REV No.	COMMENTS	DATE	INIT.
A	TENDER ISSUE	07.06.23	FS
B	TENDER ISSUE	18.10.23	FS
C	TENDER ISSUE	13.02.24	DJ
D	FOR APPROVAL	12.04.24	DJ
E	FOR APPROVAL	16.04.24	DJ
F	FOR APPROVAL	24.04.24	DJ
G	FOR S138 APPROVAL	30.04.24	DJ



**S138 APPROVAL**

**PROJECT:**  
SALLYS CORNER ROAD  
SUTTON FOREST NSW 2579

**CLIENT:**  
APS

**Richmond+Ross** PTY LIMITED  
CONSULTING ENGINEERS AND PROJECT LEADERS  
ABN 34 001 485 436





38 WILLOUGHBY ROAD,  
CROWS NEST, NSW 2065  
TEL : (02) 9490 9600  
FAX : (02) 9438 1224

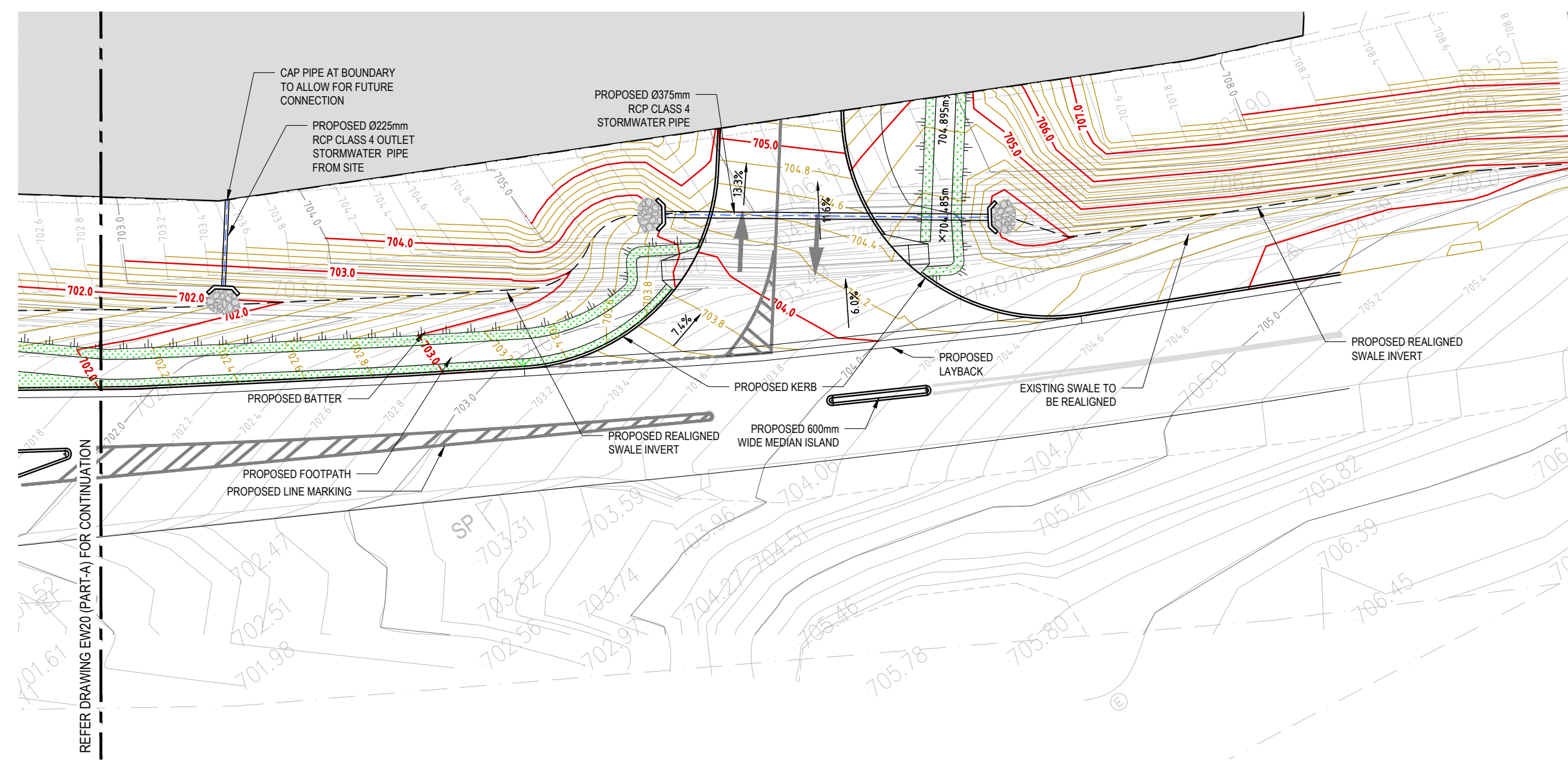
**PROPOSED SITE PLAN  
PART - A**

DATE:	MAY 2023	DRG.No.
SCALE:	1:250 @ A3	<b>EW20</b>
JOB No.	230139	REV G

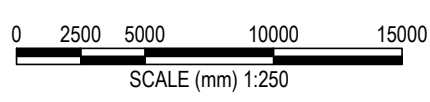


**LEGEND**

-  PROPOSED ROAD WIDENING
-  LANDSCAPE
-  PROPOSED BATTER
-  PROPOSED HEADWALL WITH ROCKFILLED WIRE MATTRESS



REV No.	COMMENTS	DATE	INIT.
A	TENDER ISSUE	18.10.23	FS
B	TENDER ISSUE	13.02.24	DJ
C	FOR APPROVAL	12.04.24	DJ
D	FOR APPROVAL	16.04.24	DJ
E	FOR APPROVAL	24.04.24	DJ
F	FOR S138 APPROVAL	30.04.24	DJ



**S138 APPROVAL**

**PROJECT:**  
SALLYS CORNER ROAD  
SUTTON FOREST NSW 2579

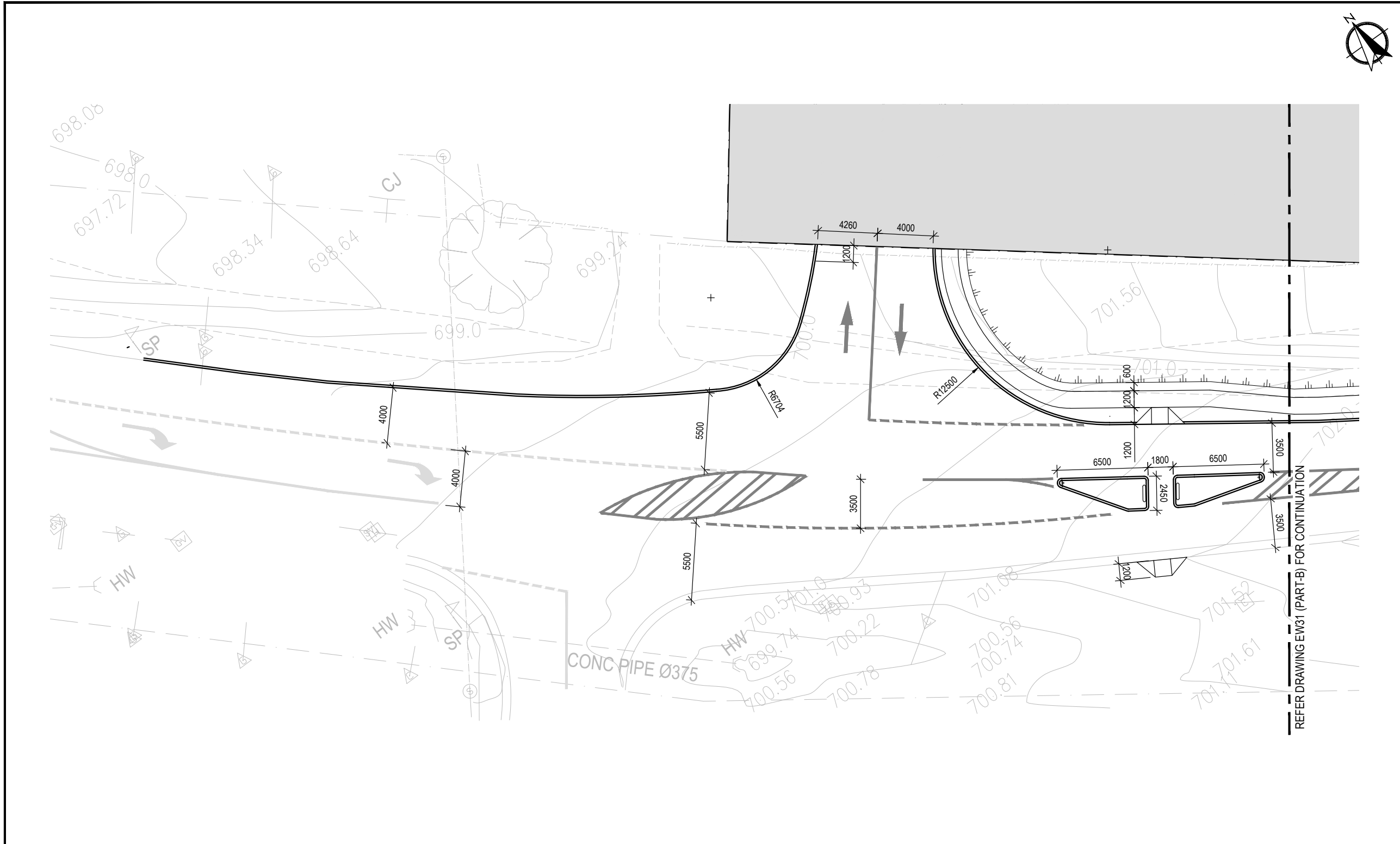
**CLIENT:**  
APS

**Richmond+Ross** PTY LIMITED  
CONSULTING ENGINEERS AND PROJECT LEADERS  
ABN 34 001 485 436

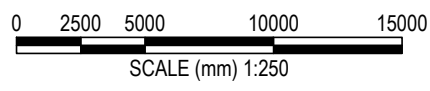
38 WILLOUGHBY ROAD,  
CROWS NEST, NSW 2065  
TEL : (02) 9490 9600  
FAX : (02) 9438 1224

**PROPOSED SITE PLAN  
PART - B**

DATE:	MAY 2023	DRG.No.
SCALE:	1:250 @ A3	<b>EW21</b>
JOB No.	230139	REV F



REV No.	COMMENTS	DATE	INIT.
A	TENDER ISSUE	18.10.23	FS
B	TENDER ISSUE	13.02.24	DJ
C	FOR APPROVAL	12.04.24	DJ
D	FOR APPROVAL	24.04.24	DJ
E	FOR S138 APPROVAL	30.04.24	DJ



**S138 APPROVAL**

**PROJECT:**  
SALLYS CORNER ROAD  
SUTTON FOREST NSW 2579

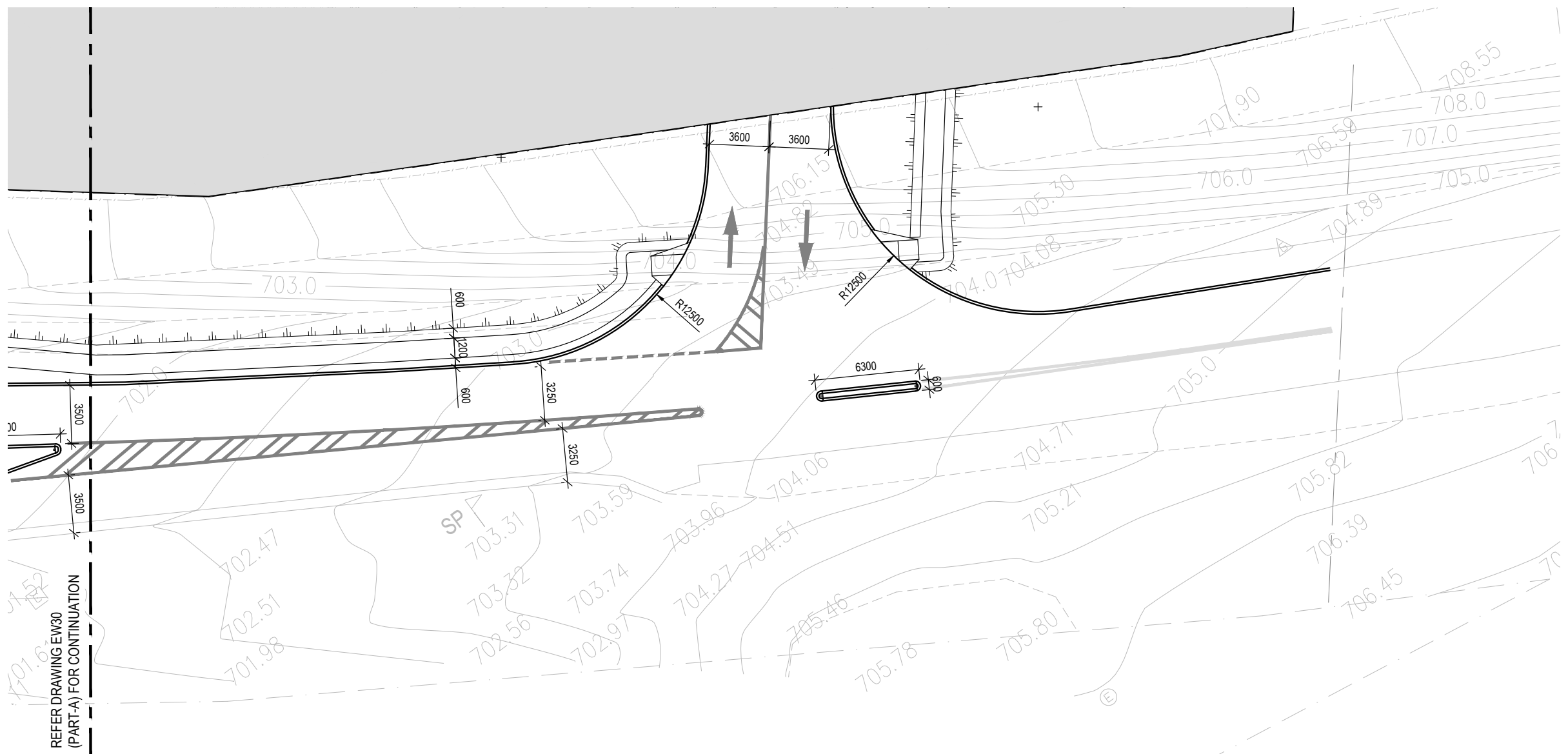
**CLIENT:**  
APS

**Richmond+Ross** PTY LIMITED  
CONSULTING ENGINEERS AND PROJECT LEADERS  
ABN 34 001 485 436

38 WILLOUGHBY ROAD,  
CROWS NEST, NSW 2065  
TEL : (02) 9490 9600  
FAX : (02) 9438 1224

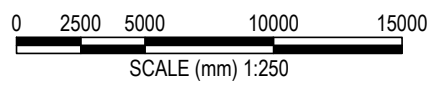
**DIMENSION/SETOUT PLAN  
PART - A**

DATE:	MAY 2023	DRG.No.	EW30
SCALE:	1:250 @ A3	REV	E
JOB No.	230139		



REFER DRAWING EW30  
(PART-A) FOR CONTINUATION

REV No.	COMMENTS	DATE	INIT.
A	TENDER ISSUE	18.10.23	FS
B	TENDER ISSUE	13.02.24	DJ
C	FOR APPROVAL	12.04.24	DJ
D	FOR APPROVAL	24.04.24	DJ
E	FOR S138 APPROVAL	30.04.24	DJ



**S138 APPROVAL**

PROJECT:  
SALLYS CORNER ROAD  
SUTTON FOREST NSW 2579

CLIENT:  
APS

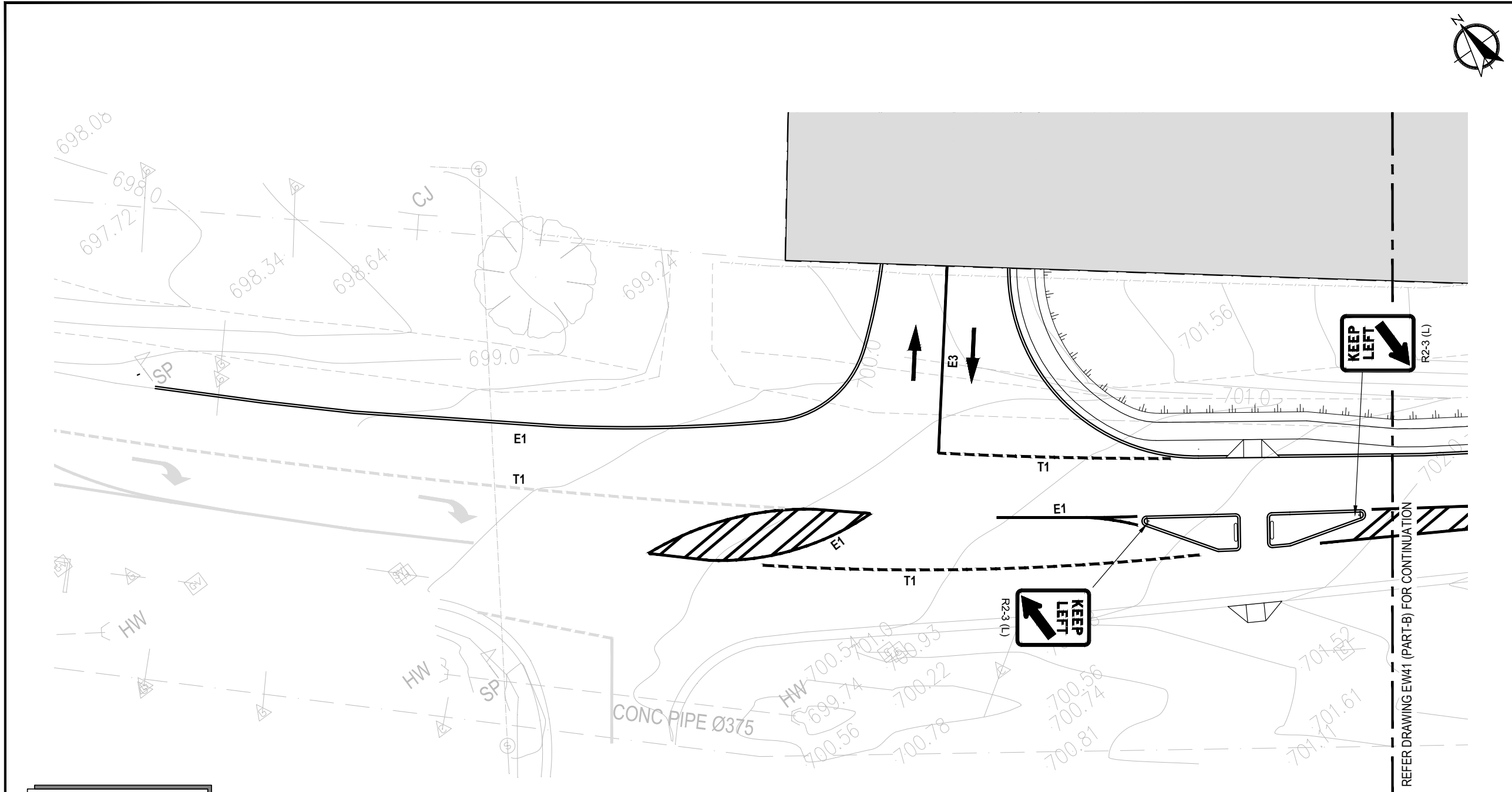
**Richmond+Ross** PTY LIMITED  
CONSULTING ENGINEERS AND PROJECT LEADERS  
ABN 34 001 485 436

38 WILLOUGHBY ROAD,  
CROWS NEST, NSW 2065  
TEL : (02) 9490 9600  
FAX : (02) 9438 1224

DIMENSION/SETOUT PLAN  
PART - B

DATE:	MAY 2023	DRG.No.	
SCALE:	1:250 @ A3	<b>EW31</b>	
JOB No.	230139	REV	E

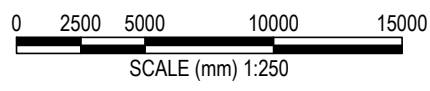




LEGEND	
E1, E3	EDGE LINEMARKING
T1	TURNING LINEMARKING

- NOTE:**
1. REFER TfNSW DELINEATION GUIDELINE SECTION 4.
  2. EXISTING LINEMARKING SHOWN IS BASED ON HISTORICAL ARIEL IMAGERY. SALLYS CORNER ROAD HAS RECENTLY BEEN RESURFACED AND AS SUCH THE REVISED LINEMARKING MAY VARY FROM WHAT IS SHOWN ON PLAN. CONTACT ENGINEER PRIOR TO CONSTRUCTION OF EXISTING LINEMARKING ON SITE VARIES FROM EXISTING LINEMARKING ON PLANS.
  3. PROPOSED EXTENSION OF THE 60 ZONE FURTHER TO THE EAST UNDER REVIEW BY TfNSW.

REV No.	COMMENTS	DATE	INIT.
A	TENDER ISSUE	18.10.23	FS
B	TENDER ISSUE	13.02.24	DJ
C	FOR APPROVAL	12.04.24	DJ
D	FOR APPROVAL	16.04.24	DJ
E	FOR APPROVAL	17.04.24	DJ
F	FOR APPROVAL	24.04.24	DJ
G	FOR S138 APPROVAL	30.04.24	DJ



**S138 APPROVAL**

PROJECT:  
SALLYS CORNER ROAD  
SUTTON FOREST NSW 2579

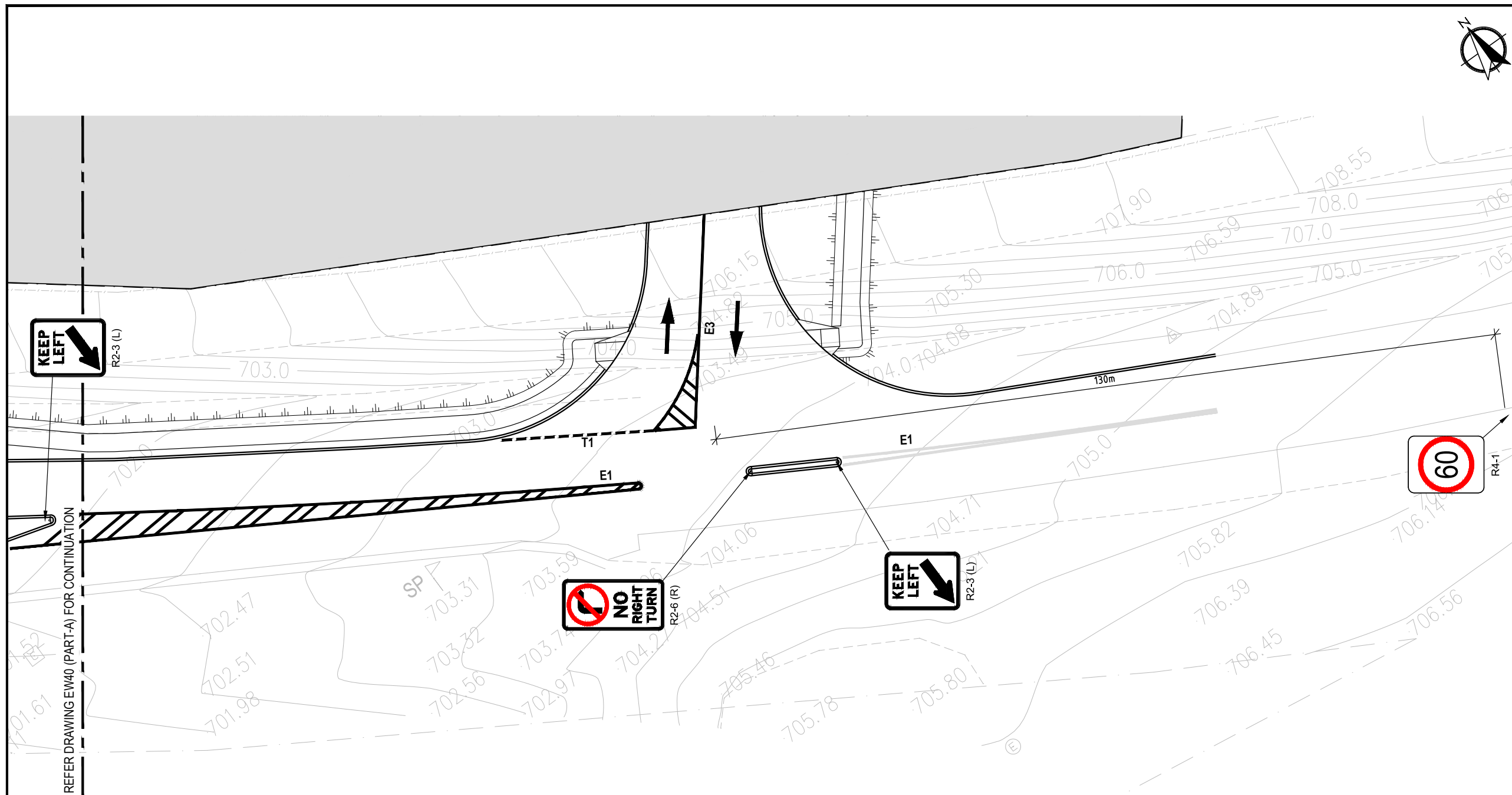
CLIENT:  
APS

**Richmond+Ross** PTY LIMITED  
CONSULTING ENGINEERS AND PROJECT LEADERS  
ABN 34 001 485 436

38 WILLOUGHBY ROAD,  
CROWS NEST, NSW 2065  
TEL : (02) 9490 9600  
FAX : (02) 9438 1224

LINE MARKING AND SIGNAGE  
PLAN - PART - A

DATE:	MAY 2023	DRG.No.	
SCALE:	1:250 @ A3	<b>EW40</b>	
JOB No.	230139	REV	G



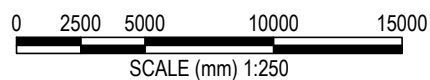
REFER DRAWING EW40 (PART-A) FOR CONTINUATION

**NOTE:**

1. REFER TfNSW DELINEATION GUIDELINE SECTION 4.
2. EXISTING LINEMARKING SHOWN IS BASED ON HISTORICAL ARIEL IMAGERY. SALLYS CORNER ROAD HAS RECENTLY BEEN RESURFACED AND AS SUCH THE REVISED LINEMARKING MAY VARY FROM WHAT IS SHOWN ON PLAN. CONTACT ENGINEER PRIOR TO CONSTRUCTION OF EXISTING LINEMARKING ON SITE VARIES FROM EXISTING LINEMARKING ON PLANS.
3. PROPOSED EXTENSION OF THE 60 ZONE FURTHER TO THE EAST UNDER REVIEW BY TfNSW.

LEGEND	
E1, E3	EDGE LINEMARKING
T1	TURNING LINEMARKING

REV No.	COMMENTS	DATE	INIT.
A	TENDER ISSUE	18.10.23	FS
B	TENDER ISSUE	13.02.24	DJ
C	FOR APPROVAL	12.04.24	DJ
D	FOR APPROVAL	16.04.24	DJ
E	FOR APPROVAL	17.04.24	DJ
F	FOR APPROVAL	24.04.24	DJ
G	FOR S138 APPROVAL	30.04.24	DJ



S138 APPROVAL

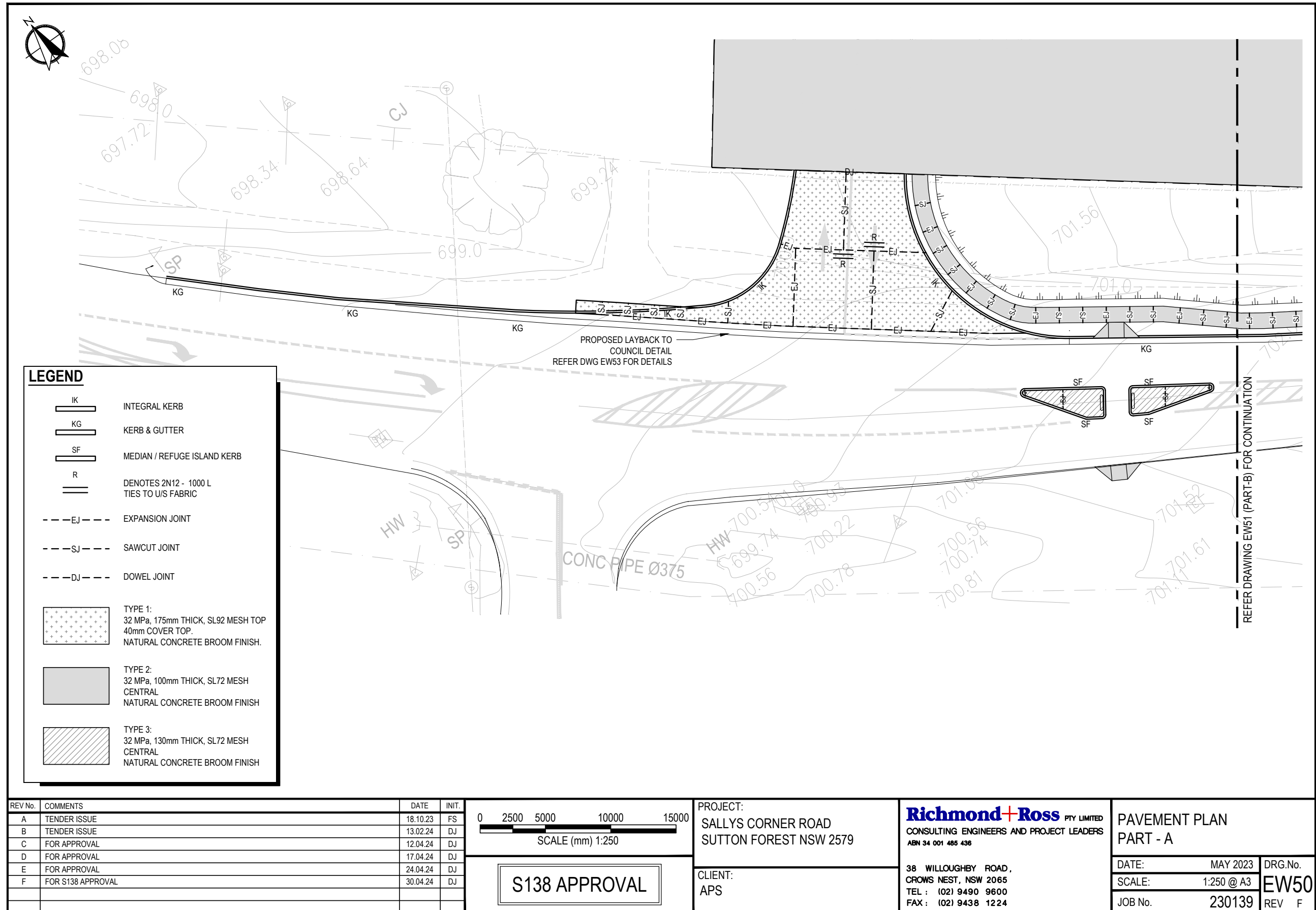
PROJECT:  
SALLYS CORNER ROAD  
SUTTON FOREST NSW 2579

CLIENT:  
APS

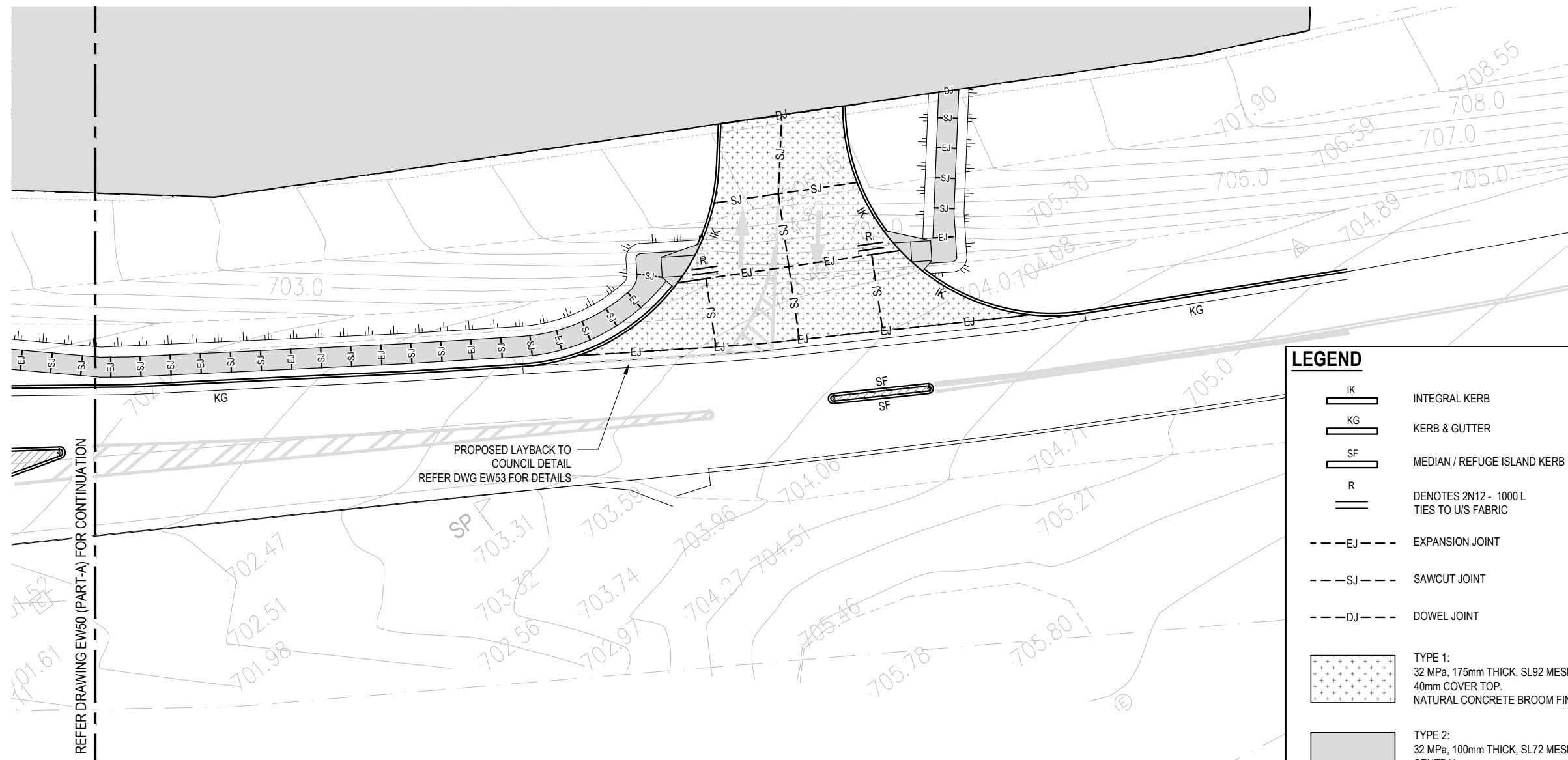
**Richmond+Ross** PTY LIMITED  
CONSULTING ENGINEERS AND PROJECT LEADERS  
ABN 34 001 485 436

38 WILLOUGHBY ROAD,  
CROWS NEST, NSW 2065  
TEL : (02) 9490 9600  
FAX : (02) 9438 1224

LINE MARKING AND SIGNAGE PLAN - PART - B		
DATE:	MAY 2023	DRG.No.
SCALE:	1:250 @ A3	<b>EW41</b>
JOB No.	230139	REV G

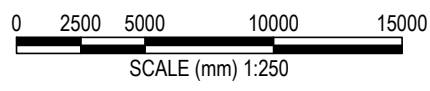






LEGEND	
	INTEGRAL KERB
	KERB & GUTTER
	MEDIAN / REFUGE ISLAND KERB
	DENOTES 2N12 - 1000 L TIES TO U/S FABRIC
	EXPANSION JOINT
	SAWCUT JOINT
	DOWEL JOINT
	TYPE 1: 32 MPa, 175mm THICK, SL92 MESH TOP 40mm COVER TOP. NATURAL CONCRETE BROOM FINISH.
	TYPE 2: 32 MPa, 100mm THICK, SL72 MESH CENTRAL NATURAL CONCRETE BROOM FINISH
	TYPE 3: 32 MPa, 130mm THICK, SL72 MESH CENTRAL NATURAL CONCRETE BROOM FINISH

REV No.	COMMENTS	DATE	INIT.
A	TENDER ISSUE	18.10.23	FS
B	TENDER ISSUE	13.02.24	DJ
C	FOR APPROVAL	12.04.24	DJ
D	FOR APPROVAL	24.04.24	DJ
E	FOR S138 APPROVAL	30.04.24	DJ



**S138 APPROVAL**

PROJECT:  
SALLYS CORNER ROAD  
SUTTON FOREST NSW 2579

CLIENT:  
APS

**Richmond+Ross** PTY LIMITED  
CONSULTING ENGINEERS AND PROJECT LEADERS  
ABN 34 001 485 436

38 WILLOUGHBY ROAD,  
CROWS NEST, NSW 2065  
TEL : (02) 9490 9600  
FAX : (02) 9438 1224

PAVEMENT PLAN  
PART - B

DATE:	MAY 2023	DRG.No.	
SCALE:	1:250 @ A3	<b>EW51</b>	
JOB No.	230139	REV	E

**PAVEMENT NOTES**

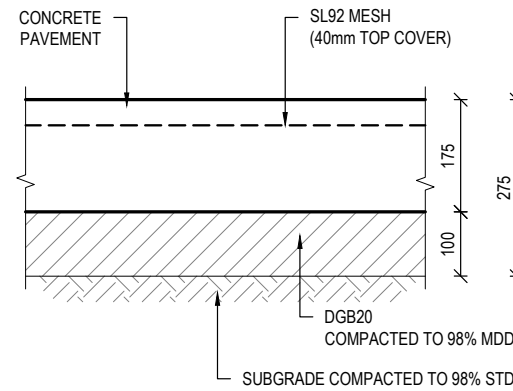
1. PAVEMENT MATERIALS SHALL BE IN ACCORDANCE WITH TNSW SPEC 3051 : BASE COURSE DGB20
2. FILL MATERIALS WHICH ARE PRONE TO ACCELERATED WEATHERING WILL NOT BE ACCEPTED EG. SOME MUDSTONES, CLAYSTONES, SILTSTONES, SHALES AND OTHER ROCKS. ENDORSEMENT OF THE SUITABILITY OF THE PROPOSED FILLING MATERIAL IS TO BE MADE BY A GEOTECHNICAL ENGINEER PRIOR TO APPROVAL. FILL MATERIALS USED TO SUPPORT PAVEMENTS SHALL BE COMPACTED TO 95% STANDARD MAXIMUM DRY DENSITY UPTO 0.5M BELOW SUBGRADE LEVEL.
3. BASE COURSE SHALL BE COMPACTED TO 98% MODIFIED MAXIMUM DRY DENSITY AT A MOISTURE CONTENT WITHIN 2% OF STANDARD OPTIMUM, MINIMUM SOAKED CBR 80% UNO.
4. SUB BASE COURSE SHALL BE COMPACTED TO 95% MODIFIED MAXIMUM DRY DENSITY, MINIMUM SOAKED CBR 45% UNO.
5. SUBGRADE SHALL BE APPROVED NATURAL SUBGRADE OR IMPORTED FILL. PROOF ROLL AND COMPACTED TO 98% STANDARD MAXIMUM DRY DENSITY UNO.

**CONCRETE PAVEMENT NOTES**

1. CONCRETE.
  - 1.1. ALL EXTERNAL CONCRETE SLABS TO HAVE A MINIMUM COMPRESSIVE STRENGTH OF 32MPa @ 28 DAYS (U.N.O.)
  - 1.2. ALL PEDESTRIAN TRAFFIC PAVEMENTS TO BE A MINIMUM OF 28MPa @ 28 DAYS
2. PROVIDE A HEAVY DUTY MEMBRANE UNDER ALL BUILDING SLABS AND TANK FARM PAVEMENTS.
3. ALL REINFORCEMENT SHALL BE SUPPORTED ON BAR CHAIRS AND DISCS - 50mm MIN COVER TO LIGS, CROSS BARS AND ALL REINFORCEMENT EXPOSED TO AIR OR AGAINST GROUND.
4. NO WATER SHALL BE PERMITTED TO BE ADDED TO PREMIXED CONCRETE ON SITE.
5. EACH TRUCK SHALL BE CHECKED FOR SLUMP USING A STANDARD CONE. SPECIFIED SLUMP TO BE 85MM ± 15MM. CONCRETE OUTSIDE THIS RANGE SHALL BE REJECTED.
6. ALL CONCRETE SHALL BE COMPACTED USING A VIBRATOR OR TRAVELLING SCREED.
7. MINIMUM TRAFFICABLE CURE TIME FOR CONCRETE SLAB AS FOLLOWS:
  - 7.1. 32MPa, 4MPa FLEXURAL = 7 DAYS UNLESS ACCELERATED WITH TNSW COMPLIANT MIX IN WHICH CASE MAY TRAFFICKED 12 HOURS AFTER CURING
  - 7.2. 60MPa = 4 DAYS (NOT SUITABLE FOR USE WITH ACCELERANTS)
8. ALL SAWN JOINTS SHALL BE CUT USING A 'SOFT CUT' 4 HOURS AFTER POUR.
9. PROVIDE AT LEAST 48 HOURS NOTICE TO THE ENGINEER FOR INSPECTION OF REINFORCEMENT, PRIOR TO POUR.
10. MAINTAIN COVER TO ALL REINFORCEMENT IN ACCORDANCE WITH THE "REINFORCEMENT DETAIL".
11. USE ONLY GALVANISED DOWEL BARS (MIN 250 GRADE).
12. SEAL ALL JOINTS (U.N.O.) IN ACCORDANCE WITH RELEVANT JOINT SEAL DETAIL
13. THE EVAPORATION RATE SHALL BE MONITORED BY THE CONTRACTOR DURING CONCRETING OPERATIONS UNTIL SUCH TIME AS CURING COMMENCES. IF CONTROL MEASURES ARE NOT SUCCESSFUL OR ARE IMPRACTICAL, NO CONCRETE SHALL BE POURED.
14. WHERE WEATHER CONDITIONS ON SITE INDICATE AN EVAPORATION RATE FROM THE CONCRETE SURFACE IS LIKELY TO EXCEED 0.50 KG/M<sup>2</sup>/H, THE USE OF EVAPORATION RETARDANT MIST SPRAYS ON THE CONCRETE SLAB EXPOSED SURFACE ARE MANDATORY. SPRAYS MUST BE MIXED IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS AND APPLIED WITHIN 10 MINUTES OF CONCRETE PLACEMENT AND INITIAL LEVELLING. SPRAYS ARE THEN APPLIED AGAIN FOLLOWING ANY SUBSEQUENT FLOATING OPERATION ON THE CONCRETE SURFACE.

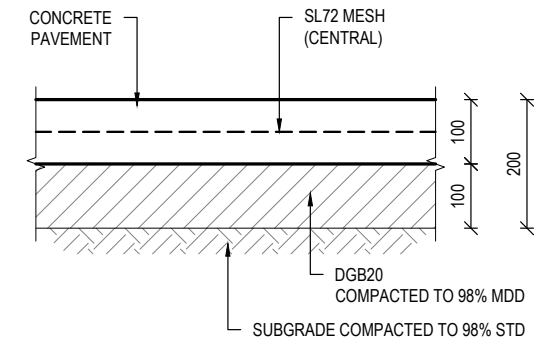
**PAVEMENT DESIGN CBR**

PAVEMENTS DESIGNED USING AN ASSUMED CBR OF 4.5% PAVEMENT PROFILES SUBJECT TO REVIEW ON COMPLETION OF GEOTECH INVESTIGATION



**TYPE 1: HEAVY DUTY  
CONCRETE PAVEMENT**

SCALE 1:10



**TYPE 2: CONCRETE PAVEMENT  
FOR PEDESTRIAN ACCESS**

SCALE 1:10

REV No.	COMMENTS	DATE	INIT.
A	TENDER ISSUE	18.10.23	FS
B	FOR APPROVAL	15.04.24	JK
C	FOR S138 APPROVAL	30.04.24	JK

0 100 200 400 600  
SCALE (mm) 1:10

**S138 APPROVAL**

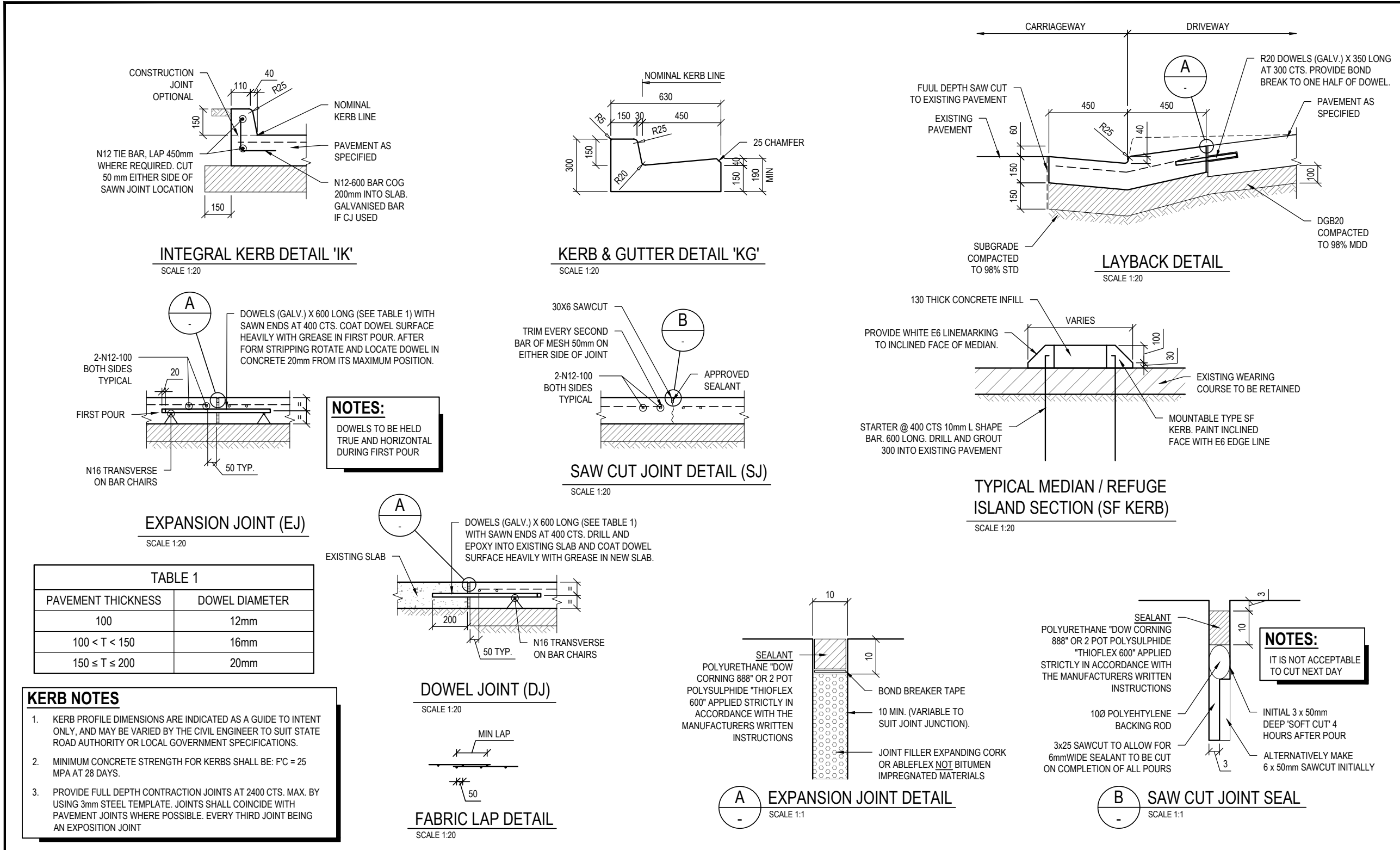
PROJECT:  
SALLYS CORNER ROAD  
SUTTON FOREST NSW 2579

CLIENT:  
APS

**Richmond+Ross** PTY LIMITED  
CONSULTING ENGINEERS AND PROJECT LEADERS  
ABN 34 001 485 436

38 WILLOUGHBY ROAD,  
CROWS NEST, NSW 2065  
TEL : (02) 9490 9600  
FAX : (02) 9438 1224

PAVEMENT DETAILS SHEET 1 OF 2	
DATE:	MAY 2023
SCALE:	1:10 @ A3
JOB No.	230139
DRG.No.	EW52
REV	C



REV No.	COMMENTS	DATE	INIT.
A	TENDER ISSUE	18.10.23	FS
B	TENDER ISSUE	13.02.24	DJ
C	FOR APPROVAL	12.04.24	JK
D	FOR S138 APPROVAL	30.04.24	JK

0 100 200 400 600  
SCALE (mm) 1:10

**S138 APPROVAL**

PROJECT:  
SALLYS CORNER ROAD  
SUTTON FOREST NSW 2579

CLIENT:  
APS

**Richmond+Ross** PTY LIMITED  
CONSULTING ENGINEERS AND PROJECT LEADERS  
ABN 34 001 485 436

38 WILLOUGHBY ROAD,  
CROWS NEST, NSW 2065  
TEL : (02) 9490 9600  
FAX : (02) 9438 1224

**PAVEMENT DETAILS  
SHEET 2 OF 2**

DATE:	MAY 2023	DRG.No.
SCALE:	AS SHOWN @ A3	<b>EW53</b>
JOB No.	230139	REV D



## **7 DATE OF NEXT MEETING**

The next meeting will be held on Thursday 18<sup>th</sup> July 2024 in the Council Chambers, Civic Centre, Elizabeth Street, Moss Vale commencing at 10am.

## **8 MEETING CLOSURE**